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John McGarry



CARRIAGE TOWN

crossroads of american industry

CARRIAGE TOWN MASTER PLAN

Prepared for the City of Flint
Department of Community Development

Mayor James A. Sharp

Support funding by the Charles Stewart Mott Foundation
Ruth Mott Fund, The Flint Public Trust and the City of Flint

TOMBLINSON, HARBURN YURK & ASSOCIATES, INC.
PROJECT ARCHITECT

TOWNSCAPE INSTITUTE
PUBLIC ART PLANNERS

June, 1984

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INTRODUCTION

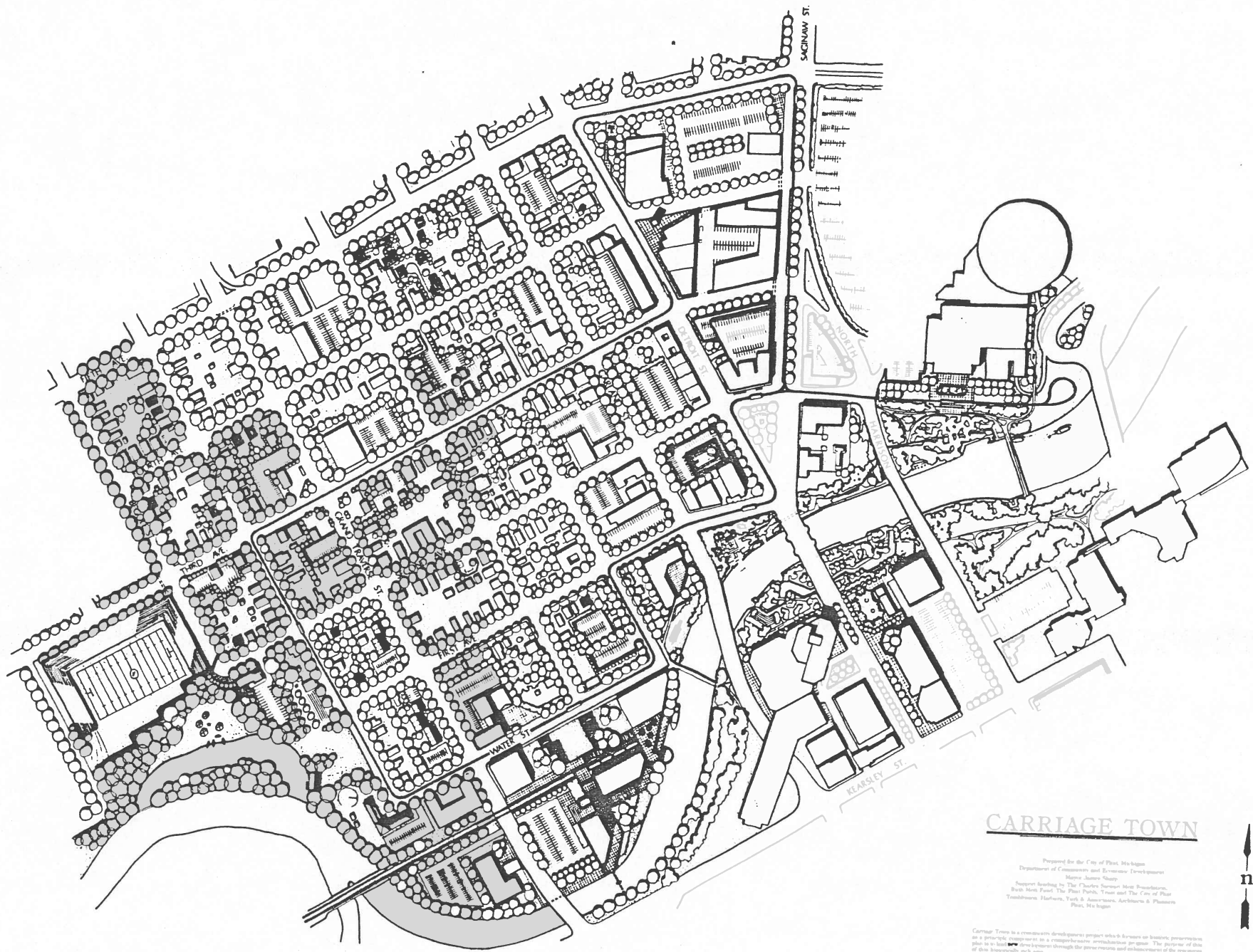
Carriage Town is an area whose streets and buildings tell a remarkable and fascinating story. It is a story of time, people and events that dramatically shaped this nation's history. In some ways the history of Carriage Town is a microcosm of urban development; typical of many northern cities. Here history spans the centuries from its simple beginnings as a river crossing along one of the oldest North American Indian trails to fur trading, lumbering, and finally an industrial manufacturing center. In other ways the history of Carriage Town is unique and unmatched in American history. Here history also spans the incredible careers of a group of men with unusual abilities and visions. "And the spark plug of them all was Billy Durant." Starting with \$2,000 of borrowed money, Durant and his partner, J. Dallas Dort, began a carriage manufacturing business that in less than twenty years would become the world's largest. This same carriage business would in time conceive and nurture a new enterprise, one that would dwarf earlier achievements by becoming the world's largest industrial empire - General Motors. The town known nationally as the 'Vehicle City' long before automobiles, became the cradle of American automotive pioneers. Automotive legends such as Durant, Dort, Walter P. Chrysler (Chrysler Corporation), Charles W. Nash (American Motors), Albert Champion (AC Spark Plug), Louis and Arthur Chevrolet and David Buick were people with visions; people who put the nation on wheels and made Carriage Town the crossroads of American industry.

The Carriage Town master plan respects this rich heritage and is conceived as a progression of history from the Indian Center and village to the Union Industrial Savings Bank and its role in the 1929 stock market crash. Carriage Town is the confirmation of the development under the dome at AutoWorld. Under the direction of this plan, we believe an important part of American history will be preserved and a community revitalized.

PRINCIPLE RECOMMENDATIONS OF THE CARRIAGE TOWN MASTER PLAN

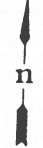
1. The creation of a local historic district as defined by the boundaries of the Carriage Town Master Plan and a National Registered Historic District(s) as deemed appropriate by the State History Division and Flint Historic Commission.
2. The establishment of a Carriage Town Council that would serve as a liason with the various coalitions impacting on Carriage Town and the prime development authority for the area. The Council should be comprised of representatives from the: Merchants Association (2), Residents Association (2), AutoWorld Management (1), Genesee County Indian Association (1), Historical Society (1), Flint Historic Commission (1), Planning Commission (1), Department of Community Development (1), F.E.A.T. (1) and several representatives from the community at large (Arts Council, Visitors Bureau, Horse Drawn Vehicle Association, etc.)
3. The appointment of a Carriage Town Development Authority is to coordinate and accomplish the various development and promotional tasks on a day-to-day basis. Initially this could be within the frame work of DCD but should eventually seek support funding and direction from the Carriage Town Council.
4. The initiation of a systematic plan to locate and assist appropriate businesses and development to locate within the appropriate areas.
5. The establishment of a highly visible law enforcement program within the area.
6. The creation of Carriage Town Square as a viable festival/special events area.
7. The development of several different and distinctive modes of transportation for sightseeing and short distance travel: i.e. horse drawn carriages; rehabilitated autos (pre 1930); electric trolleys; pleasure boats.
8. The application of the U.S Department of the Interior Standards for Historic Preservation and the Design Guidelines of the Flint Historic Commission for building projects within the Carriage Town area.
9. The reclamation and improvement of the residential areas.
10. The joint promotion of events and attractions. The packaging of multi-activities on a single ticket: for example one level of admission to AutoWorld would include a round trip tour of Carriage Town.

11. The development of a 'placemaking program' as suggested by Townscape Institute to enrich the experience and awareness of both visitor and resident.
12. The encouragement of pedestrian access and movement throughout Carriage Town.
13. The establishment of a major 'western anchor' to compliment AutoWorld on the east.
14. The development of a public area site improvement program that is coordinated with various city departments and conforms to the revitalization plan.
15. The exercise of restraint during promoting and marketing of Carriage Town to insure the appropriate development and businesses in this highly desirable area.
16. The retaining of the existing street grid patterns.



CARRIAGE TOWN

Prepared for the City of Port. Oregon
 Department of Commission and Economic Development
 Harry James Sharp
 Support provided by The Charles S. Barrett Trust Foundation,
 Bank Street Fund, The First Pacific Trust and The Cecil B. Starr
 Foundation. Portland, Ore. & Associates, Architects & Planners
 Port, Oregon



Carriage Town is a community development project which focuses on historic preservation as a principal component in a comprehensive revitalization project. The purpose of this plan is to lead the city through the process of re-evaluation and enhancement of the character of this historically rich area.

The Carriage Town Tax is introduced to provide the incentives for the private sector to encourage the preservation of historic structures and the revitalization of the area. No public use of properties is anticipated or intended. It is expected that the City of Port's primary role will be to provide public utility site improvements if and when funding becomes available.

FORMAT

The Carriage Town master plan report is organized to bring a natural order to a complex and varied urban area. Nine distinct development areas are identified and comprise Carriage Town; they are: AUTOWORLD, INDUSTRIAL MUTUAL ASSOCIATION PLAZA, NEON DISTRICT, RIVER MARKET, CARRIAGE FACTORY DISTRICT, MERCANTILE DISTRICT, RESIDENTIAL QUARTER, URBAN FARM AREA AND ANISHINABE AKI. Each development area is present in the following format:

RESOURCE

The resources, both tangible and intrinsic, of the area are identified and expanded upon to establish the frame work for future development.

IMAGE DESCRIPTION

An overview of the recommended development plans for the area describes the fundamental character, activities and attractions that are envisioned.

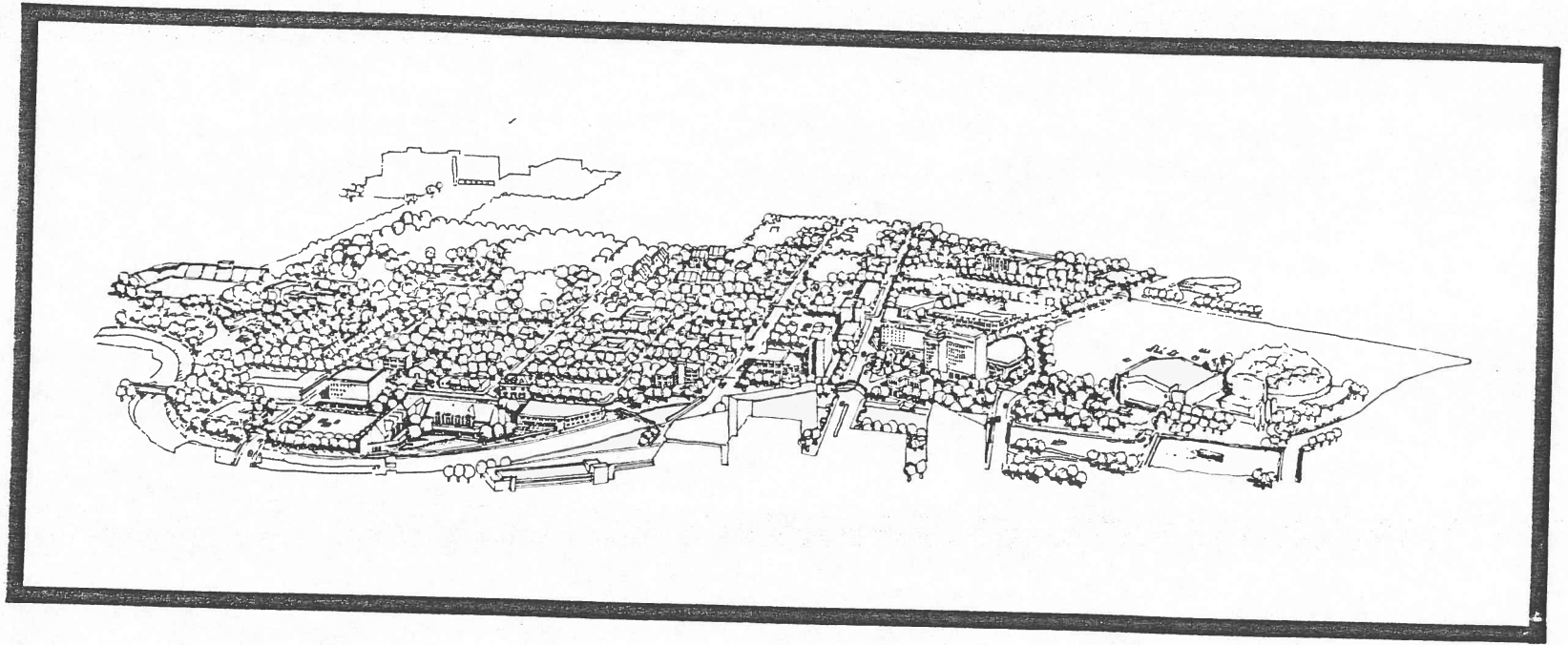
COMPONENTS

The various components that contribute to the desired character of the area are identified and further refined in their development.

RECOMMENDATIONS

Guidelines are presented that will direct the development of the area for the desired character and finished product.

AUTOWORLD

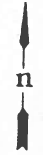




AUTOWORLD

CARRIAGE TOWN

Prepared for the City of Flint, Michigan
 Department of Community and Economic Development
 Mayor James Sharp
 Support provided by The Charles Stewart Mott Foundation,
 Bank Home Fund, The Flint Public Trust and The City of Flint.
 Tomlinson, Hudson, York & Associates, Architects & Planners
 Flint, Michigan



Carriage Town is a community development project which focuses on historic preservation as a viable component in a comprehensive revitalization program. The purpose of this plan is to lead new development through the preservation and enhancement of the structure of this historically rich area.

The Carriage Town Master Plan is intended to provide the framework for the private sector to encourage the preservation of historic structures and the revitalization of the area. No public acquisition of property is anticipated or implied by the Master Plan. The City of Flint's primary role will be to provide public uses and improvements if and where feasible to make available.

March, 1984

RESOURCE

• EXISTING TOURIST ATTRACTION

AutoWorld is a tourist oriented theme park focusing on the history and future of the automobile. It is a multi-faceted place of fun and fact, of leisure and learning. Projected to draw nearly one million visitors each year, AutoWorld is the eastern anchor of Carriage Town and the first major visitor attraction in the district.

• CARRIAGE TOWN INTRODUCTION

AutoWorld provides an entertaining introduction to automotive history and the pioneers of the industry who put the nation on wheels. A recreation of early Flint sets the stage for the introduction of "a group of men of unusual ability ... not a more lively bunch anywhere else in the country"¹. It provides an excellent introduction to the personalities that lived and worked in Carriage Town.

• RIVERBANK PARK

Riverbank Park begins at this point and has attractions such as an Archimedean screw and the water duct. Hamilton dam is the dividing point between high and low river levels. Potential exists to provide for water attractions and transportation. Riverbank Park itself provides a pedestrian oriented path to Flint's CBD and Carriage Town.

¹George H. Maines, Picture History of Flint.

IMAGE DESCRIPTION

The predominant character of this area is a transition space between the theme creations within AutoWorld and the reality of the actual historic sites portrayed. Here the Autoworld visitor is alerted and drawn from the exit of AutoWorld to the river and points west. It is an orientation area, a place for the AutoWorld visitor to catch their breath and plan further adventures. It is a major transportation center and transfer point for travel between AutoWorld and other points of interest throughout the city. Horse drawn carriages, restored period autos, trolley cars and turn of the century replica pleasure boats on the river will provide a wide range of transportation choices for individual preference, convenience and finances.

COMPONENTS

• AUTOWORLD PLAZA

The AutoWorld entrance/exit plaza is a brick/aggregate paved plaza with landscaping and a chronological display of autos. Beginning at AutoWorld's door with the latest models (perhaps on loan from the manufacturers) and progressing to the river with older models. The display would capture the memories of the motoring public. Not the vintage, rare or milestone cars but the popular everyday cars which revive memories in us all. The cars that were part of our childhood, part of our dating years, the first car owned ... activating our memory bank and leading us down a path to new adventures.

- CARRIAGE TAXI STAND

Carriage taxi stand - visible from the entrance to AutoWorld. Curb side service provides next-in-line hire for tour or transportation. Drivers could be independent operators licensed by the city or private company drivers.

- TROLLEY STATION TURNAROUND

Provides both a sight and sound attraction that calls people to ride the rails in restored trolleys similar to the ones which served Flint at the turn-of-the-century, "until the automobile gradually took over". The trolley would connect AutoWorld with major historic sites and attractions throughout Carriage Town.

- ANTIQUE AUTO TAXI RENTAL

Restored autos serve visitors as the horse drawn carriages do. For maximum freedom families could pile into rehabilitated period autos and drive their own route through Carriage Town.

- PLEASURE BOAT CRUISES

River travel is made possible on both sides of Hamilton Dam with turn-of-the-century pleasure boat recreations. Travel upstream could transport tourists to remote Autoworld parking sites and for tours at the Buick and Dupont complexes at Hamilton Avenue. Travel downstream will transport people to the Carriage Factory District.

RECOMMENDATIONS

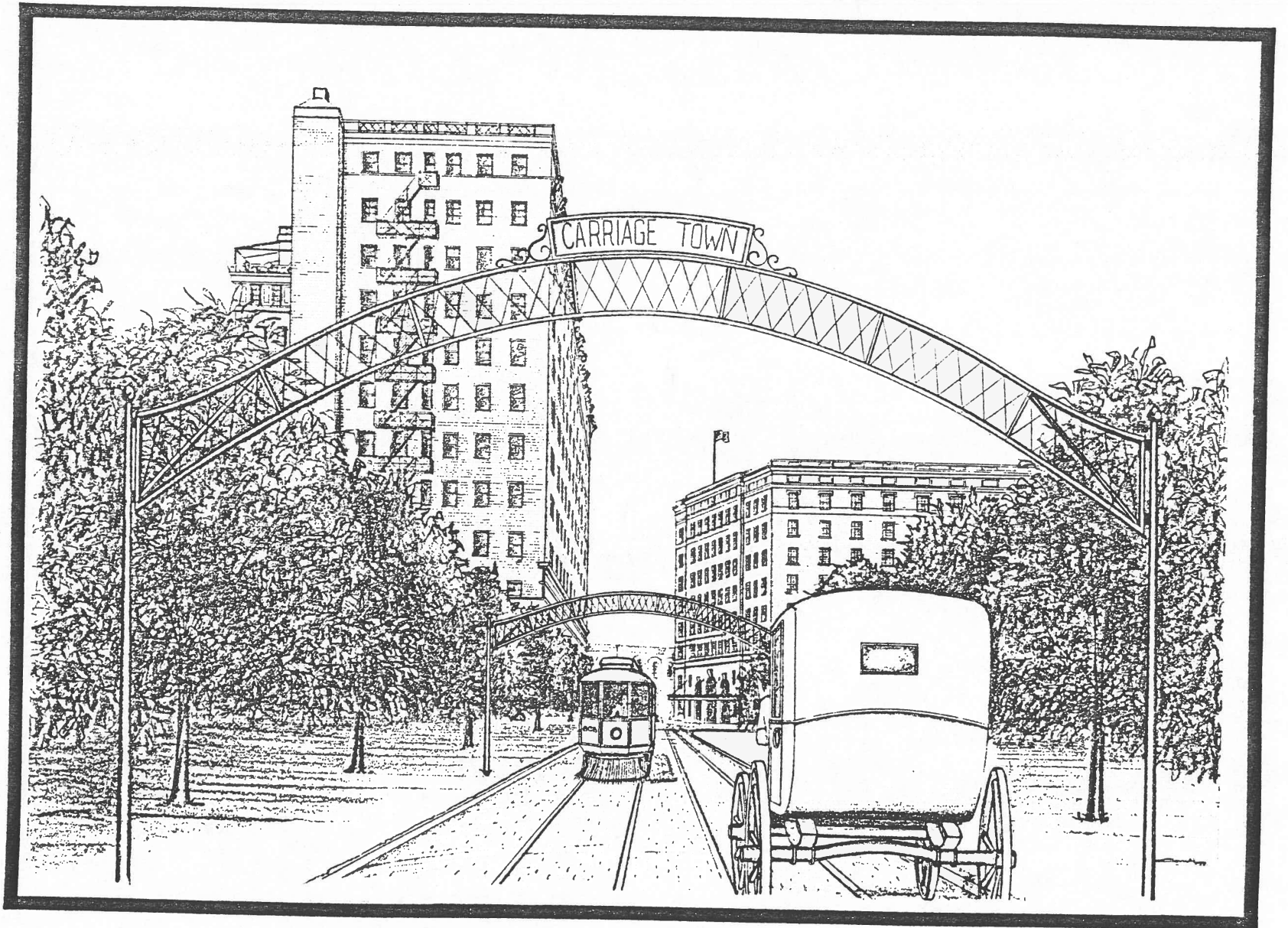
- AUTHENTICITY

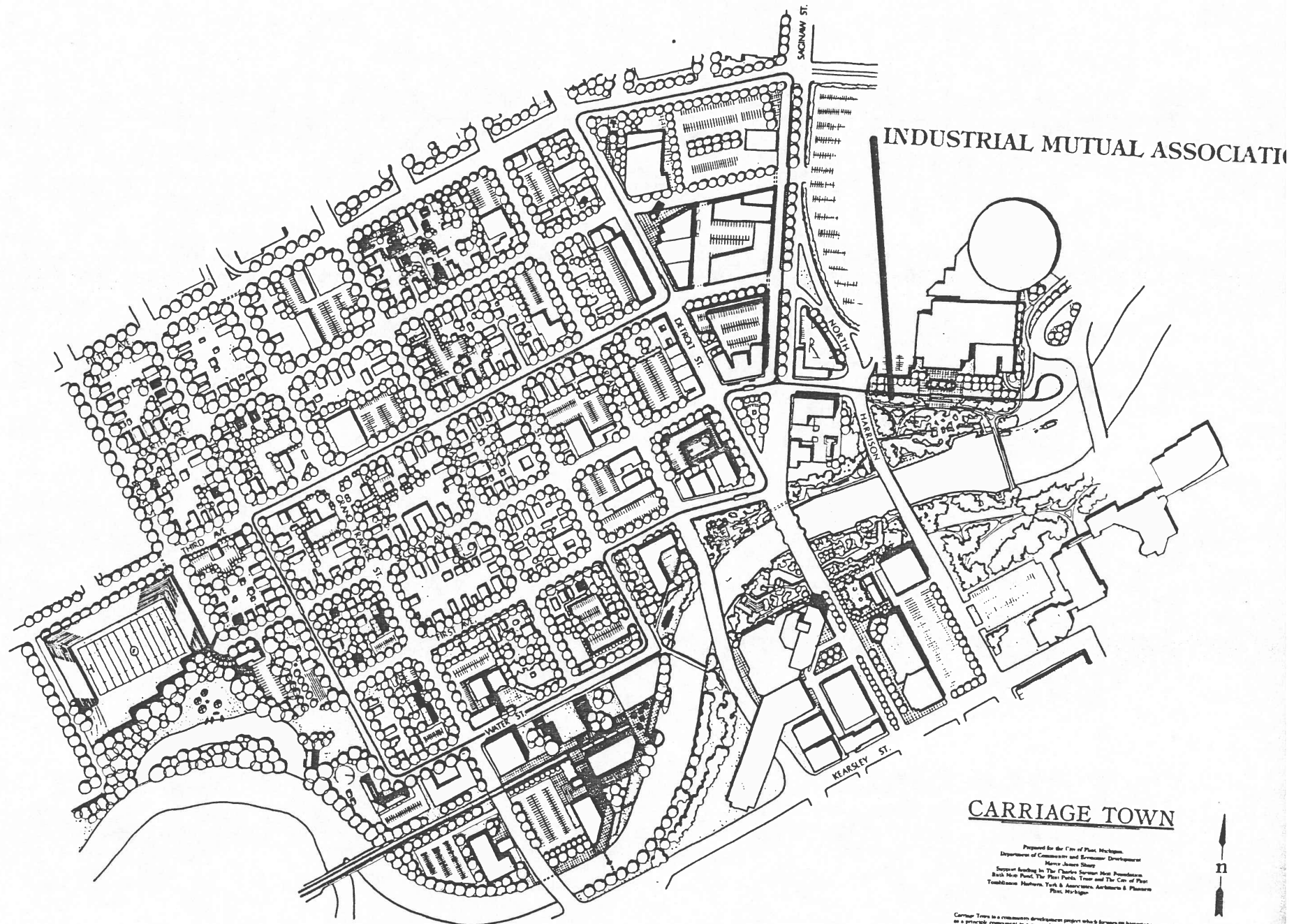
All plan elements should strive for factual and bonafide development to convey to the visitor/user a reliable, legitimate operation. Authenticity becomes an attraction itself. The clanging trolley - sound of steel against steel rail, the sputter of overhead electric lines and the bouncing ride becomes an experience to relive time and again.

- MARKETING

Package multi-activities on a single ticket to encourage exposure and full usage of the attractions. For example, one level of admission to AutoWorld could also include a round trip of Carriage Town via one of the modes of transportation.

IMA PLAZA





INDUSTRIAL MUTUAL ASSOCIATION

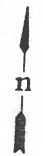
CARRIAGE TOWN

Prepared for the City of Flint, Michigan,
 Department of Community and Economic Development
 Marie-Jane Sharp
 Support funding by The Charles Sargent May Foundation,
 Bank New York, The Flint Park, Trust and The City of Flint,
 Tomlinson Hardware, York & Associates, Architects & Planners
 Flint, Michigan

Carriage Town is a community development project which furthers an historic preservation as a principle component in a comprehensive revitalization program. The purpose of this plan is to assist and guide the process through the preservation and enhancement of the resources of this historically rich area.

The Carriage Town Master Plan is intended to provide the framework for the process and to encourage the preservation of historic structures and the revitalization of the area. No public acquisition of property is anticipated as required by the Master Plan. The City of Flint's primary role will be to provide public area site improvements if and where funding becomes available.

March, 1984



RESOURCE

• HISTORIC SIGNIFICANTS

The Industrial Mutual Association Auditorium (IMA) is a site of historical significance to industrial and labor history. The IMA resulted from the turn-of-the-century amalgamation of several benefit societies that offered workers the equivalent of modern day health and life insurance.

• ARCHITECTURAL RESOURCE

The IMA (Industrial Mutual Association) Auditorium provides a valuable resource on a visually prominent site along the city's waterfront. The stately, classic lines of this large 1929 Italian Romanesque structure are preserved, contributing to the character of Carriage Town's eastern edge.

• HIGH VISABILITY

Highly visible AutoWorld/Carriage Town Face that will aid in locating AutoWorld from several desirable locations - Hyatt Regency, Riverfront Park, CBD, State Office Building, U of M-Flint.

• DRAMATIC VIEWS OF FLINT'S SKYLINE

The IMA Plaza in return offers one of the best views of the Flint skyline and would enlighten AutoWorld visitors to a city beyond their immediate environment.

IMAGE DESCRIPTION

The preservation of the historic facade of the IMA provides a stately backdrop to the plaza that provides display areas for small periodical auto shows and conventions. The plaza has a commemorative theme to the industrial worker and serves as an attraction itself, luring AutoWorld visitors on a path to Carriage Town. A brick paved road passes through the southern edge of the plaza conveying horse drawn carriages, restored trolleys, antique autos and pedestrian traffic. A landscaped park area encloses the plaza on the sides with the southern edge merging into the Riverbank Park.

COMPONENTS

• IMA PLAZA

A formal, symmetrical water fountain complements the stately facade of the IMA Auditorium. The wide fountain walls provide an accommodating seat for people wanting to draw close to the sight and sound of the water. The row of flag poles unfurling their color and motion again proves its people attraction power in the IMA Plaza. Night lighting highlights the flags and the IMA facade identifies this area and AutoWorld during non-operation hours from several advantage points.

- LIMITED ACCESS STREET

The 15-20 feet wide brick paved street is a limited access street serving the transportation center at AutoWorld and service vehicles. A straight concrete curb provides edge definition between the street, the park and plaza.

- ARCHES

Recreations of "Flint Vehicle City" arches span the street in arcade fashion in the park area. The arches continue the theme within AutoWorld to the outside, demonstrating that history doesn't end at the gates of AutoWorld.

- PUBLIC ART

A sound generating/motion sculpture of moving gears, wheels, horns, etc. is a participatory sculpture in Riverbank Park near the river.

Immediately in front of the restored IMA Auditorium a hard surface paver provides a suitable surface for this heavy pedestrian area. As a commemorative plaza to the American industrial worker, lifesize sculptures will depict Flint's industrial development of lumbering, carriage manufacturing and automobile manufacturing.

RECOMMENDATIONS

- PLAZA DESIGN

The plaza should have two distinct areas. One, the hard surface workers commemorative/display area with fountain and flags symmetrically designed in front of the IMA Auditorium. Two, the landscaped park area that links the AutoWorld site and plaza with Riverbank Park.

- LANDSCAPING

Landscaping should screen the employee parking area and new construction on the west and north side of the IMA. A formal landscape plan in this area should emphasize the IMA facade and reinforce the formal design of the hard surface area.

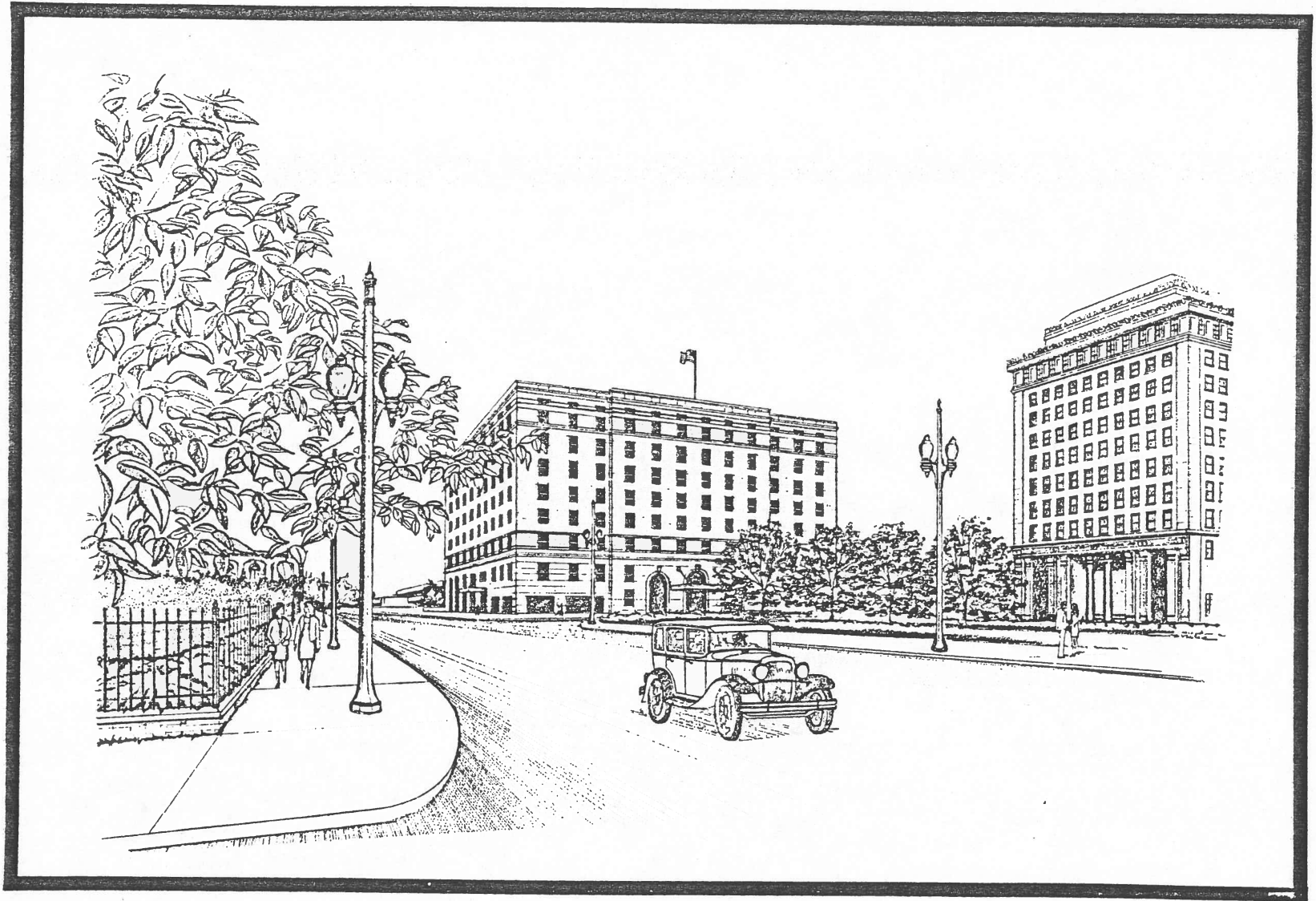
Landscaping south of the brick street would be less formal and subtly merge into Riverbank Park.

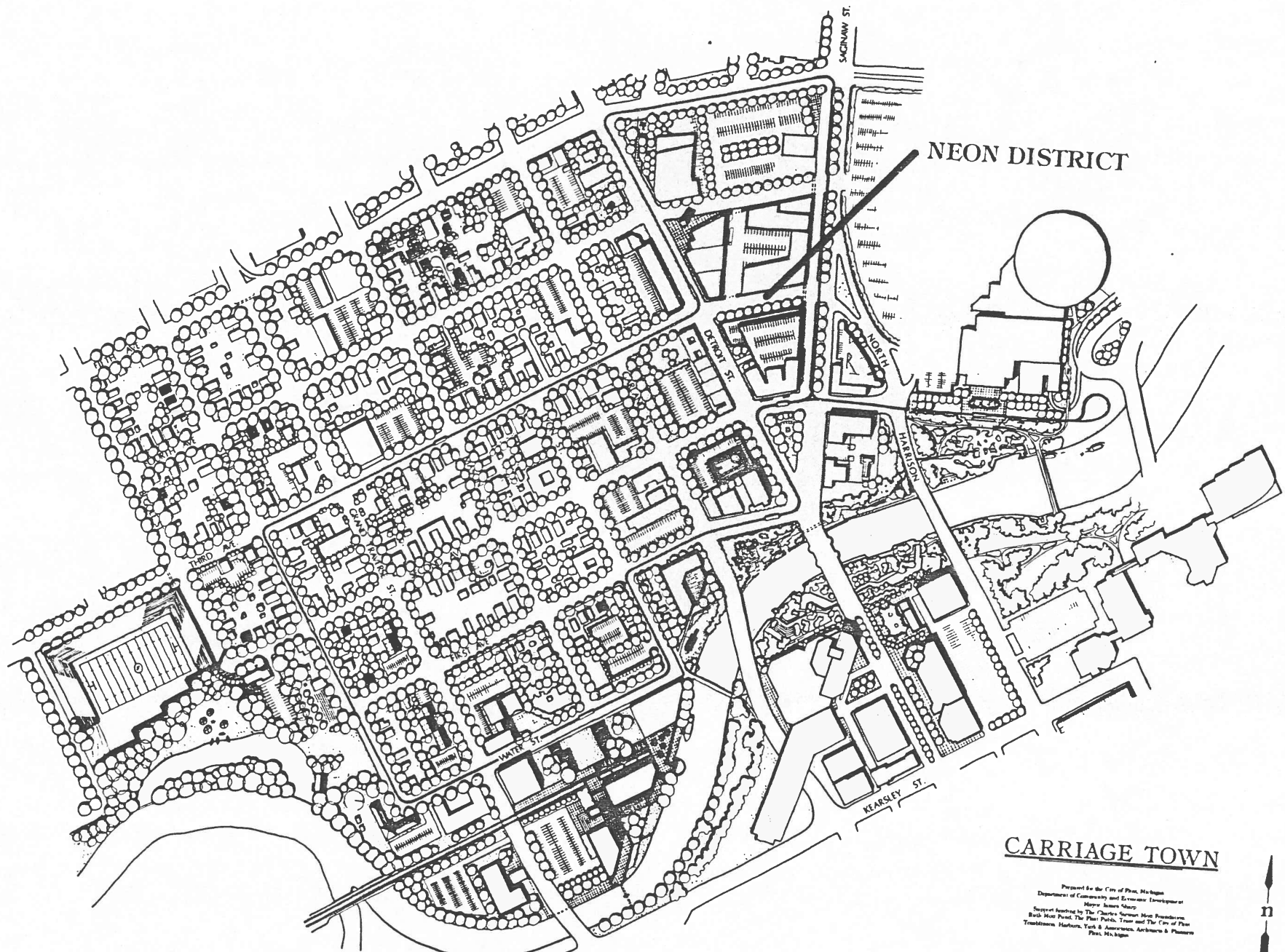
Design and maintenance of the landscaping should provide a clear, unobstructed focus on the IMA and formal plaza.

- ARCHES

The "Flint Vehicle City" arches should occur in the park areas and be closely spaced (40-50 feet) to provide an impression of a canopy over the street.

NEON DISTRICT





NEON DISTRICT

CARRIAGE TOWN

Prepared for the City of Flint, Michigan
 Department of Community and Economic Development
 Mayor James Wherry
 Support Funding by The Charles Stewart Mott Foundation
 Bob Mott Fund, The Flint Parks, Trust and The City of Flint
 Transbarn, Harburn, York & Associates, Architects & Planners
 Flint, Michigan

Carriage Town is a community development project which focuses on historic preservation as a primary component in a comprehensive revitalization program. The purpose of the plan is to lead area development through the preservation and enhancement of the resources of the historically rich area.

The Carriage Town Master Plan is intended to provide the framework for the project and to encourage the preservation of historic structures and the revitalization of the area. The study, acquisition of property as equipment or needed by the Master Plan. The City of Flint a primary role will be to provide public use site improvements if and where funding becomes available.

March, 1984

RESOURCE

• HISTORICALLY SIGNIFICANT STRUCTURES

Several large and historically significant structures dating from the 1920's exist in this district. Structures such as the Union Industrial Bank, Durant Hotel, Buick Factory Showroom, Italia Gardens (1920's Pontiac Dealership) and Fire Station Number 2 all have National Register potential and possess a common theme in Flint's development.

• COHERENT STREETScape

Within this district other architectural resources exist which depict the phenomenal growth and styles of the 1920's. A number of structures: Marion Hall; Berridge Hotel; College Inn Hotel, the College Inn; North Bank Place (Consumers Power); Herrlich's Warehouse; Michelin/Firestone dealer at 4th and Saginaw; the former service station at 4th and Detroit and several of the single family houses contribute to the 1920's character of this district.

• STRATEGIC LOCATION

The proximity of this district to AutoWorld, the Hyatt Regency Hotel, Water Street Market and Windmill Place offers a unique opportunity to provide the connecting link for these projects.

• POTENTIAL HOTEL DISTRICT

Several former hotels remain in the area and could be reopened as hotels to compliment the Hyatt Regency and advance the convention trade.

• PUBLIC AMENITIES

Two important public parks, McFarlan and Hamilton, have a long established presence in the area and can provide an attractive landscaped area. Riverfront Park and the Grand Fountain is a resource of great economic value and attraction ability.

• HIGH VISIBILITY AREA

Topography and existing vistas make this area a primary focal point from the central business district, Windmill Place and several other vantage points.

IMAGE DESCRIPTION

The metropolitan flavor of the 1920's predominates the streets and architecture of this area. By night, neon lighting, symbol of American energy and showmanship in the spirited 'roaring 20's', is creatively used to produce a unique and unifying element along the streetscape. By day, bright color storefront awnings become the predominate and unifying feature. The booming and prosperous city which came of age with the automobile can be revisited. The world of high finance, dream makers and flamboyant lifestyles come alive in the hotels, restaurants, stores, offices and streets that comprise this metropolitan commercial center.

COMPONENTS

. NEON LIGHTING

The rediscovery of neon lighting to create distinctive silhouettes of figures, letters and/or buildings will play an important role in the identity of this district. Just as neon lighting transformed American cities into nighttime wonderlands during the 1920's and 30's, it once again becomes a symbol of American energy, inventiveness, showmanship and identity for this district.

. CONTINUOUS STOREFRONT FABRIC AWNINGS

A unified streetscape during the day is created with the help of storefront awnings of uniform color, character and height. The awnings provide the daytime equivalency of the nighttime neon.

. A RESTORED DURANT HOTEL

The restoration of the Durant Hotel as a hotel accurately recovers the original character found here in the 1920's. From guest room to lobby to bellhop uniforms, the historic character is recovered in detail. Broad market appeal lures guests and visitors to stay at the hotel built by Durant or to dine and be entertained in the Grand Ballroom. A multi-level deck provides convenient parking for hotel guests and visitors to Carriage Town.

. A RESTORED INDUSTRIAL SAVINGS BANK

The restoration of the Industrial Bank Building (Metropolitan Building) returns this historic site to full utilization once again. The "Skylark Club" can be revived as a restaurant offering the most advantageous panorama view of Flint. The former gymnasium and locker rooms can serve as a private health club for office tenants or public membership. The former bank vault in the basement could be adaptively used as a "speak-easy" nightclub complete with peephole admission.

. A RESTORED PONTIAC DEALERSHIP

Adaptive re-use of the Pontiac Dealership (Italia Gardens) would preserve and restore the exterior of this 1920's structure. Continual use as a restaurant would capitalize on the automotive theme. The auto motif would draw from the building's own history and AutoWorld's theme. The structure is ideally located along the major vehicle route from the IMA Plaza so as to capture the interest of AutoWorld/Carriage Town tourists. Its architectural style also provides an appropriate introduction into this 1920's district. A multi-level deck designed to blend in with the architecture of the Pontiac Dealership provides convenient parking for diners, hotel guests and the Industrial Bank Building. Underground passage could directly link parking to adjacent structures.

- A RESTORED BUICK FACTORY OUTLET

A restored Buick Sales and Service Building with its large showroom windows fronting AutoWorld parking could once again provide showroom space for new Buicks. Area car dealers could also occupy sales space on an individual permanent basis or as a rotating joint arrangement. Additional space uses that would be of interest to the AutoWorld and Carriage Town visitor could include an antique auto restorer, antique auto parts and accessory dealer, auto trades, referral service, Buick and GM Information Center, factory tour center, Flint Area Tourist Information Center, etc.

- PRESERVATION OF FIRE STATION NO. 2

This public safety facility continues to serve this area as it has since the 1920's. Future plans for this building should be one of preservation and adaptive use when this facility is replaced with a new station further north.

- FACADE IMPROVEMENTS

Sensitive and discriminating facade renovation of existing non-supporting architecture blends with the preservation/restoration of contributing architecture.

- PUBLIC AREA SITE IMPROVEMENTS

Public area site improvements include street resurfacing, restoration of period lighting, street accessories, landscaping and park improvements.

RECOMMENDATIONS

- BUILDING RESTORATION/PRESERVATION

Revitalization of the NEON District needs to include the preservation and restoration of the contributing structures in the district. Included are all National Register potential structures and all contributing structures identified as part of the COHERENT STREETScape. Facade improvements should conform to the Secretary of the Interior's Standards and the Design Guidelines of the Flint Historic Commission.

- NON-SUPPORTING ARCHITECTURE MODIFICATION

Existing non-supporting architecture should be modified to be more compatible with the district. A building facade should not be altered to look 'historic' but to better integrate with its neighbors and support the character of the district. Several key design elements will greatly influence the visual character: color, proportion, material and, signage. Example of an appropriate architectural modification is provided in the appendix.

- COLOR: Earth tone colors predominate the buildings in this district. Non-supporting architecture should select colors in the same range as the unaltered material colors of supporting architecture. Color combinations should be limited and used in a decorative, contrasting manner. Trim, for example, many times was lighter than the main building color. Shine and metallic finishes are not compatible with the district's image.

. PROPORTION: The relation between width and height of the existing facades should be respected. Proportions of doors and windows are also important. Windows and door arrangements should not emphasize the horizontal but occur as single spaced openings particularly on the upper floors. The facade at street level should incorporate the transparent 'storefront' openings that can be seen on the major period architecture in the area. Roofs should be flat and not extend or give the appearance of extending beyond the building. Canopies at entrances should be bracketed off the building. Fabric awnings are recommended at street level.

. MATERIAL: Solid and durable appropriately describes the character of materials. Common face brick, cut stone with straight edges etc. are recommended. Siding and panels such as metal, wood, plastic, porcelain, etc. should not be used.

. SIGNAGE: The character of this district allows flexibility in signage. The use of neon is highly recommended to create the unifying element and hence the name of this 1920's district. Signs which use incandescent bulbs that outline letters or borders are also appropriate signage of the 1920's. Letter styles should be selected from the sans serif, serif face or script styles. Library review of period magazines and newspapers will also aid businesses in appropriate sign selection. Sign placement on the building is also important and should be limited to:

- . Under the lower cornice
- . On the awning flap
- . Hanging from the building (Herrlich's Drug Store, see example)
- . Painted on the glass

Examples of appropriate signage and letter styles can be found in the appendix.

. NEW CONSTRUCTION

New construction should be compatible with the 1920 character of this district. Exterior materials should be restricted to standard brick (4" x 8" x 2-3/4") as facade material. Design detail should be achieved by position of the brick (rowlock, soldier courses, corbels, etc.), window openings should be 'punched openings' in single or double unit combinations occurring between structural bays which are expressed in masonry on the facade. Cut stone used as trim, bands, and relief would be appropriate.

Parking decks should use interior ramps and not sloped decks in order to allow level horizontal expression of the facades. Retail/commercial use of the ground level should be encouraged and be pedestrian oriented towards the streets. The use of shallow display windows resembling storefront windows rented by merchants for advertising may be a practical alternative to actual commercial space. Brick and limestone trim should be the principle facade material.

When built adjacent to existing structures, such as the Hotel Durant, the deck should maintain the first two levels of limestone with similar openings. The upper levels would also follow the upper hotel levels using brick, punched openings and limestone trim.

Height limitations of two to three stories should be in effect along the western edge (Garland Street) to form a transition between the residential quarter and the Neon District.

Reference of new construction in the Secretary of Interiors Guidelines for rehabilitation also provides good general guidelines.

• NEON LIGHTING

Neon lighting should be encouraged for both information signage and visual art. The building facades could also incorporate neon to highlight the architecture and direct the eye to the signage and/or entrance. The use of neon should not become gaudy as 'Las Vegas character' but retain a smaller more intimate character found in the 1920's.

• CANVAS AWNINGS

The use of bright, uniform colored awnings above storefront level is recommended to provide the daytime unifying element. Such identification will capitalize upon the visibility of the area as noted under Resources. The awnings should be fabric and uniform in color, character, height, etc.

• PRIVATE LANDSCAPING

Private landscaping efforts are somewhat limited because of the practice of building to the property line. When and where possible, private landscaping efforts should reinforce the feeling of an uninterrupted streetscape. Iron fences (not chain link or wood), low evergreen or shrub plantings, etc. would be appropriate.

• BUSINESS DEVELOPMENT

Businesses, shops and entertainment that represented the 1920 era should be encouraged to locate in this district. The following examples provide a general look at potential markets in addition to those outlined in the Components section.

- Small theaters that feature original 1920-30's screen idols (Rudolph Valentino, Al Jolson) as well as current first run movies. Special showings may feature the news and sports of the 1920's (Babe Ruth, 'Red' Grange, Jack Dempsey). Second and third run movies that are set in the 1920's (Great Gatsby).

- Bookstores, complete with Main Street (1920), Babbett (1922), Arrowsmith (1925), the Great Gatsby (1925) and Farewell to Arms (1929). Period newspapers and magazines available to the serious collector can also delight the amateur patron.

- Jazz and dance clubs combining with restaurants.

- Stores that can equip its patrons with both current and period fashions. Accessories such as hip flasks, cigarette holders, etc. could be sold for the person who has everything - a souvenir from Flint of the 1920's.

• PUBLIC AREA SITE IMPROVEMENTS

Several major improvements within the public areas which would greatly influence the revitalization of this district includes the following:

- The brick resurfacing of all streets within the district.
- The creation of an architectural facade along both sides of Detroit Street to create infill architecture at vacant lots and surface parking areas. Constructed as part of a public area site improvement program, the new facades would provide an immediate and complete streetscape in design conformance. The facade walls could become actual storefronts and be used as a marketing tool to encourage the desired businesses to locate in this district.
- The relocation of all utilities underground.
- The placement of period street lighting fixtures that duplicate the original. Two distinct styles are suggested. Twin fixtures on a flaired concrete pole should be used along

Saginaw Street from the river to 5th Avenue and along Detroit from the river north to 1st Avenue. Several original poles still exist. Single fixture with tear globe is recommended on a similar flaired pole for all other locations in the district. Modern lighting sources such as high pressure sodium could be used in conjunction with these fixtures. See recommended lighting standards in the appendix.

- The creation of a center boulevard on Saginaw Street with a major overhead canopy formed with linden trees.
- The 'non-improvement item' of retaining the original street grid pattern. Reducing curb to curb widths or alteration of straight curb is not an appropriate public improvement. The present street grid has historic presence with its relation to the Indian Reservation survey.
- The recreated 'Flint Vehicle City' arch and its placement at its historic location on the north side of the Saginaw Street bridge. With night illuminations, the arch creates a gateway entrance for Carriage Town.
- The creation of gateway entrances at 3rd and 4th Avenue intersections with Saginaw Street. Information guides (signage, paving inserts, paving graphics, etc.) in AutoWorld's parking lot informs and directs visitors to the gateway entrances and Carriage Town.

- Edge definition between 4th Street and the college parking lot would be developed with an overhead canopy of little leaf linden trees and an open iron fence commonly found on college campuses of the 1920's. The south edge of 4th Street should be defined by the existing storefront and new infill construction storefronts or screen walls sympathetic to the architecture of the period.
- The closing of Chippewa Street between Detroit and 4th Avenue to allow for additional employee parking and convenient servicing of the stores and businesses in this block.
- The development of a vertical element such as a clock tower, bell carillon (appropriate near the campus) or kiosk at the southeast corner of Detroit and 4th Streets becomes a visible destination point from Windmill Place and AutoWorld parking. The "stepping back" of the storefronts at this corner also creates a small plaza and a visual invitation to turn the corner.
- The physical expansion of Hamilton Park that results from the closing of Root Street between 5th Avenue and Detroit Street. The park expansion should retain the original curb lines and be developed as a hard surface (brick) area for street vendors and patrons.

- The rehabilitation of McFarlan Park to provide the pedestrian with an inviting, pleasant area to relax and watch the activity of this metropolitan center. The focal point of the Veterans Memorial is reinforced with contrasting paving inlaid between the directional concrete bands. Period park benches and lighting can create a distinctive and inviting atmosphere for day-long use. The attraction of this park can be immediately improved with the thinning or removal of the low shrubbery which deters usage because of visual security and easy access.

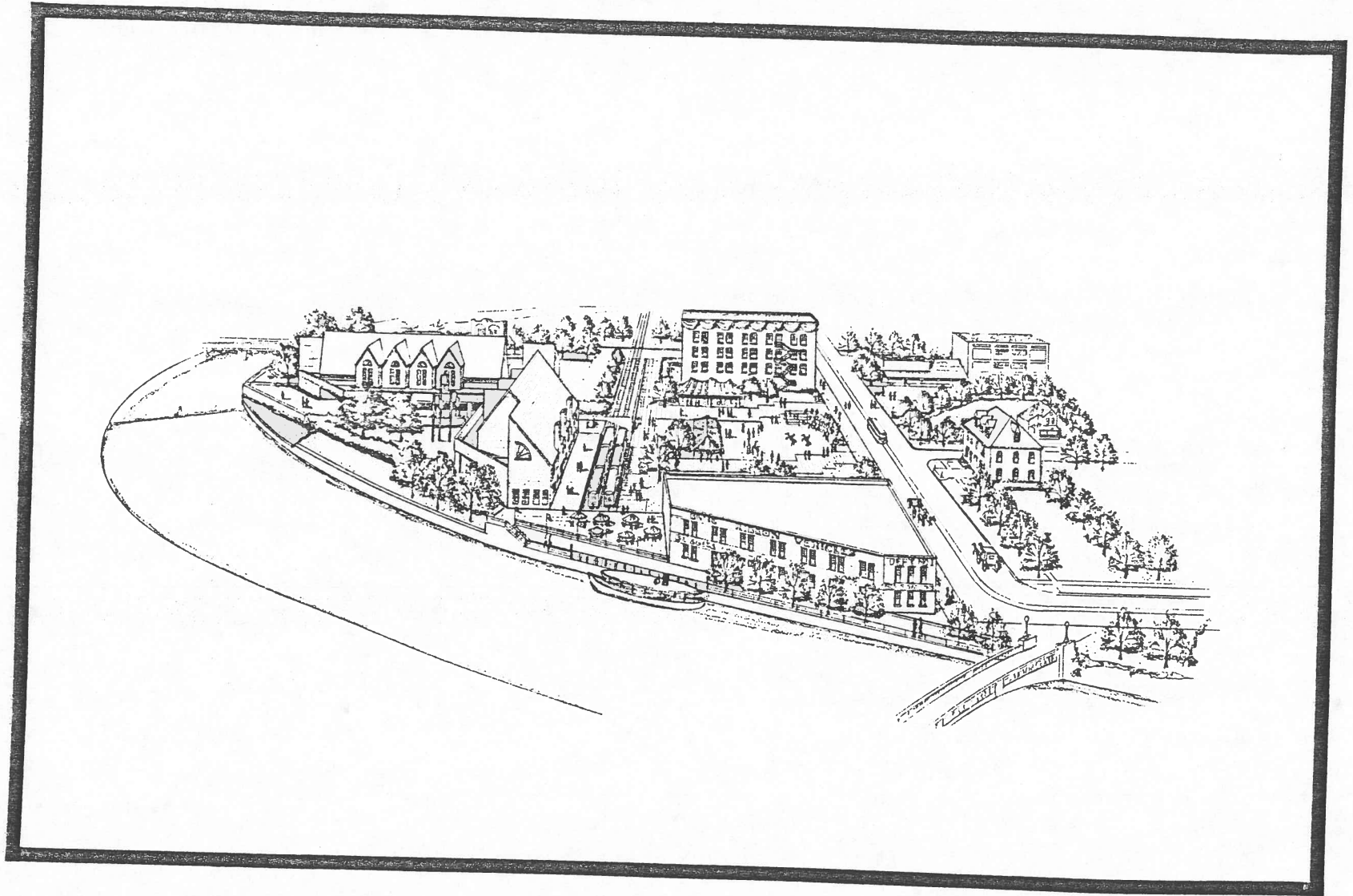
• PUBLIC ART

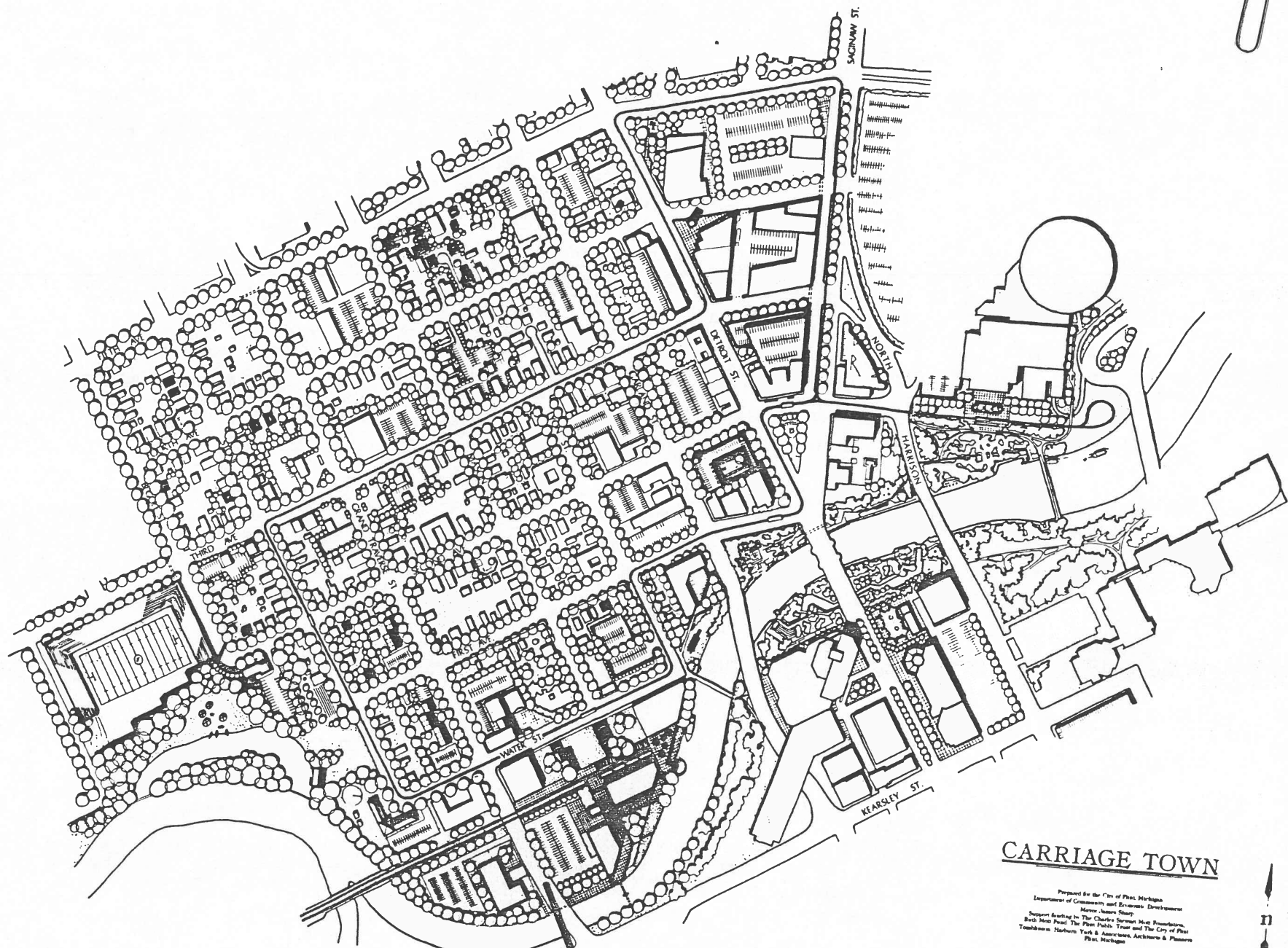
- Commission of public art should include both informational and entertaining art supportive to the character of the NEON DISTRICT. Informational art could include one or more of the following:
 - Full page newspaper reproductions of world, national and local events portraying the attitudes and history of the 1920's in an entertaining and informative way. At select locations throughout the district, people can read the news of Emperor Wilhelm's abdication, the end of World War I,

"Lucky Lindy", the Tea Pot Dome scandals, Hoover's sweep to the presidency, prohibition, bootlegging, stock reports, the business adventures of Durant, Nash, Chrysler and Buick, construction and opening of the Hotel Durant and the Industrial Bank, the stock market crash and the greatest banking embezzlement scheme in history.

- Directional art such as paving, inserts and signage which alerts and guides visitors to points of interest in Carriage Town.
- Entertaining art could include one or more of the following:
 - Sculptural art which recreates full size period autos that can be permanently parked in front of the Durant Hotel. The individual autos should be authentic and substantial enough to allow public access for photo opportunities using the hotel, Union Industrial Bank, etc. as backdrops.
 - Wall murals which develop non-supporting architecture to a more supportive role and/or downplay its intrusion in the district.

GRAND TRAVERSE VILLAGE





CARRIAGE TOWN

Prepared for the City of Flint, Michigan
 Department of Community and Economic Development
 Merce Moore Skyles
 Support funding by The Charles Sumner Mast Foundation,
 Ruth Hood Pease, The Flint Public Trust and The City of Flint
 Tomlinson, Hartman York & Associates, Architects & Planners
 Flint, Michigan

GRAND TRAVERSE VILLAGE

Carriage Town is a community development project which focuses on historic preservation as a primary requirement in a comprehensive revitalization program. The purpose of this plan is to help the area's citizens through the preservation and enhancement of the resources of this remarkable area.

The Carriage Town Master Plan is intended to provide the framework for the project to help the preservation of historic structures and the revitalization of the area. Plans are prepared and approved by the City of Flint. Plans are prepared and approved by the City of Flint. Plans are prepared and approved by the City of Flint.

RESOURCE

• RIVER ACCESS

Riverfront Park is the southern boundary of Carriage Town. A tree lined stroll along rivers' edge is possible from AutoWorld to Grand Traverse Street. Access potential exists along this walk to link the river and Carriage Town.

• MARKET FIRE STATION

The Market Fire Station, a local historic site, provides a western terminus for the river walk. Sufficient land exists for on site development to occur.

• MAJOR NORTH/SOUTH ARTERY

Grand Traverse exists as a significant north/south artery and is one of only four roadways which cross the river in Carriage Town.

IMAGE DESCRIPTION

A new contemporary development at rivers edge is conceived in alliance with the industrial history of the area, in much the same manner as New York City's South Street Seaport, San Francisco's Cannery, Boston's Quincy Market or the Torpedo Factory in Alexandria, Virginia. Here at the Flint River, a high density, mixed-use development of housing and commercial provide a vibrant market place

with consumer residents. To the west, the Market Fire Station, adaptively restored, has its own drawing capability as a children's museum or professional offices. Together these two developments bring a new vitality to this southern peninsula of Carriage Town.

COMPONENTS

• NEW DEVELOPMENT

A new multi-use complex at the former Flint Lumber Company site provides a high density, mid-rise housing with retail/commercial on the lower levels. The contemporary expression of this complex borrows from its early industrial surroundings in a fashionable way to make it a sympathetic, yet unique complex. Upper income housing in this location takes advantage of the riverfront, Carriage Town and the CBD; while bringing a consumer/resident to the immediate area.

• MARKET AREA

The Market Plaza aids in expanding Carriage Town Square to the market area and river. The plaza forms the roof of a below grade parking level. Additional surface and deck parking serves the complex on site.

. MARKET FIRE STATION

The restored Market Fire Station and new complimentary structure(s) to the west provides an ideal location for a variety of functions. Most everything from a children's museum to professional offices can draw upon the historic setting and character of the fire station. There is sufficient land to accommodate on site parking and develop related attractions, such as the locomotive train for a children's museum.

RECOMMENDATIONS

. NEW CONSTRUCTION

New construction in this area should be limited to three or four stories in height. The repetitive forms of industrial architecture should be expressed in repetitive elements in new construction - dormers, windows, chimneys, etc. Exterior materials should be limited to a random pattern brick for walls, gables and chimneys. Roofs should be wood shingle, slate or metal. Public pedestrian paths at grade level should encourage movement between the river and plaza. Three or more levels of parking would serve each of the various functions. Below grade parking would serve employees and service areas. Grade level parking immediately west of the plaza, would serve customers. Secured deck level parking would provide residents and guests direct access to the living units.

New construction at the Market Fire Station site should not obstruct the historic facade

of the fire station, but portray it as a focal point. It is recommended that new construction occur to the west and south where the historic facade has been lost to earlier remodeling. Height of new construction should be limited to that of the historic facade or less. Street setback of new construction should be greater than the existing station to further enhance the prominence of the facade. Materials should compliment the materials of the fire station. Heavy landscaping should be encouraged along the river to screen the industry further west and south.

. ADAPTIVE USE

Adaptive use of the Market Fire Station should restore the facade using the Secretary of Interior Guidelines and the Design Guidelines for the Flint Historic Commission.

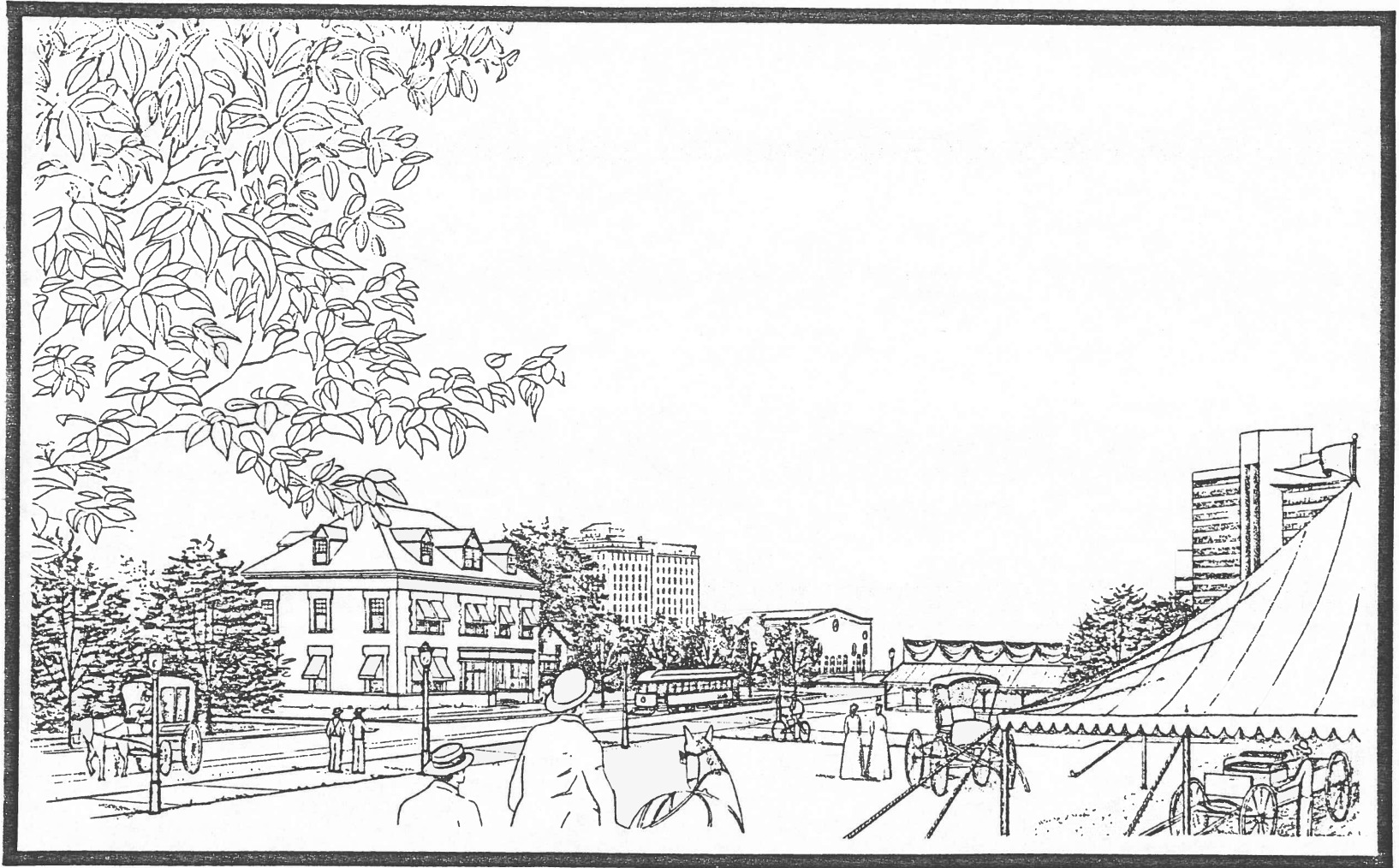
. EXPAND RIVERFRONT WALK

Sufficient land should be retained along the river at the Market Fire Station to allow for future expansion of Riverfront walk. Riverfront Park would have a definite western terminus at the Urban Farm and Anishinabe Aki.

. CARRIAGE TOWN ENTRANCE IDENTIFICATION

A Carriage Town gateway entrance with night illumination are recommended for the entry point at the Grand Traverse Street bridge.

CARRIAGE FACTORY DISTRICT



RESOURCES

. CONCENTRATED HISTORIC RESOURCES OF NATIONAL SIGNIFICANCE

Widely recognized as the hub of Carriage Town it is situated along a three block stretch at Water Street between Lyon and Stone. This is an area of exceptional historical value - it is the essence of Carriage Town and AutoWorld. The history of this area includes some of Flint's earliest industry: Stone Woolen Mills, the Flint Soap Works, Durant-Dort Carriage Company, and one of the world's largest - General Motors.

. DURANT-DORT OFFICE BUILDING

The Durant-Dort Office Building, birthplace of General Motors and headquarters for the Durant-Dort Carriage Company is a national landmark historic site and distinction reserved for only the nation's most elite historic sites such as Independence Hall and Mt. Vernon.

. ORIGINAL ROAD CART FACTORY

The first factory of the Flint Road Cart Company, forerunner of the Durant-Dort Carriage Company.

. CARRIAGE FACTORY NO. 1

Department No. 1 factory of the Flint Road Cart Company/Durant-Dort Carriage and Dort Motor Car Company.

. CARRIAGE FACTORY NO. 4

Factory No. 4 of the Durant-Dort Carriage Company and later assembly plant of the Dort Motor Company.

. OPEN SPACE

The open space between Factory No. 1 and Factory No. 4 offers excellent opportunity for visually enhancing the historic structures of the surrounding area and providing an open activity area.

. REDEVELOPMENT CATALYST

The resulting character of the restoration of the 'heart and soul' of Flint will affect to a great extent the success of the revitalization of Carriage Town and a new Flint industry - tourism. Unquestionably the most historic, the most potential and easiest to accomplish is the Water Street District. In no other area does the city have such a concentration of direct control as they do here. A determined aspiration to rediscover a pedestrian oriented street of restored green open spaces, factories and office buildings will result in a district as equally important and satisfying as colonial Williamsburg.

IMAGE DESCRIPTION

The industrial character of the 1890's and early 1900's is preserved and cultivated in the Carriage Factory District. Few areas

offer the potential and attraction that this area can have - a "Greenfield Village" in its actual setting, a restored 1900's "Williamsburg" portraying the character of an industrial town at the turn of the century.

The town known nationally as the 'Vehicle City' was to become one of the cradles of the automobile era and the birthplace of General Motors. The paths of Durant, Dort, Chrysler, Nash, Buick, Chevrolet, Champion and other automobile pioneers can once again be retraveled at this industrial center where it all began. Restored carriage factories once again buzz with activity of workers and tourist. Carriage making is revived in the heart of the 'Vehicle City'. The brick paved street with period lighting provides overflow space for Carriage Town Square, the place for events and activities.

COMPONENTS

. A RESTORED DURANT-DORT OFFICE BUILDING

The Durant-Dort office building is historically restored on both the interior and exterior. The main office area, lobby and display area on the first floor are accurately restored in a museum quality setting open to the public. The second floor provides rehabilitated office space for various community organizations that can benefit from the image of this landmark. The third floor attic space, could be used as art studio or meeting rooms for community organizations.

. A RESTORED FLINT ROAD CART/DEPARTMENT NO. 1

The original Flint Road Cart and Department No. 1 of the Durant-Dort Carriage Company occupies a prime location at the east end of Water Street. This historically important building provides visual attraction from the river, Beach Street and the Hyatt Regency. Behind the restored facade a fine dining experience awaits in the unique atmosphere of a Carriage Factory. Diners interest are attracted to the history of the building via displays, photos, menus and seating accommodations. From various seating arrangements in the restaurant views to the adjacent retail shops and actual carriage factory are possible. Seasonal dining is also available on the outside plaza overlooking the river and pleasure boat dock. During the winter months iceskaters replace diners on the plaza providing year-round activities in similar fashion to Rockefeller Center in New York.

. A RESTORED FACTORY NO. 4

A restored Factory No. 4 of the Durant-Dort complex becomes a home for new business pioneers in Flint. This four story building at Grand Traverse and Water hosts a variety of manufacturing/retail outlets compatible with the district. Inspiring young craftsmen learn from the masters in the trades of wood-working, furniture making, toy makers, weavers, textile goods, antique restorers, artists, etc. While the exterior facade is accurately restored, the interior could be modernized in a manner which accentuates the historic character and provides an attractive, interesting atmosphere for shoppers.

• CARRIAGE TOWN SQUARE

Carriage Town Square is an outdoor festival area located between Factories No. 1 and No. 4. Reminiscent of earlier village green or commons that are characteristic in New England towns, this former site of two other Durant-Dort factories is preserved as an open green enhancing the surrounding environment of these historically significant and landmark buildings. On a day-to-day basis the square serves the neighborhood residents, workers and visitors in a relaxing, informal way. At other times Carriage Town Square comes alive with the sights and sounds of events such as Blue Grass Festivals, ethnic festivals, carriage shows, flea markets, outdoor markets and the annual Carriage Town Festival.

• PRESERVATION OF THE INHERENT CHARACTER

The importance of the Water Street District and the character that should be achieved can not be over emphasized. This area should be a hub of activity and events. A sense of unity is maintained by the relation of all the buildings and Carriage Town Square. The human scale of the street, buildings, and open space is evident and should be preserved.

• LIMITED WATER STREET ACCESS

Public closing of Water Street, while impractical at the present time, should be realized in the near future (2 - 5 years). The public closing of Water Street to automobiles would further enhance the total environment of this

1890's district in much the same way as Mackinaw Island, Colonial Williamsburg or Greenfield Village total environment is amplified and is marketed.

• ARCHITECTURAL PRESERVATION/RESTORATION

The preservation/restoration of all the historic structures in the district. Included are Durant-Dort Office, Flint Road Cart Factory No. 1, Factory No. 4, Factory No. 5 and the Nash house. Preservation/restoration should conform to the Secretary of the Interior Standards and the Design Guidelines of the Flint Historic Commission.

NEW CONSTRUCTION

New construction should not be allowed along the frontage of Water Street. It is recommended that consideration be given to removing the structure at Lyon and Mason Streets to create the promenade of the Durant-Dort Office. Future considerations also might extend to the removal of the masonry structure south the Nash house. This would allow for visual linkage of all the National historic structures in the neighborhood as it once appeared at the turn of the century.

• MODIFICATIONS OF NON-SUPPORTING ARCHITECTURE

Non-supporting architecture should be integrated into the character of the neighborhood with the use of fabric awnings, signage color, lighting and landscaping. The existing buildings, north of the square and Factory #4, have rehabilitation potential. Currently only minor face lifting would be necessary to assimilate these facades into the streetscape.

• PUBLIC AREA SITE IMPROVEMENTS

Public area site improvements should include the following:

- Brick resurfacing of Water Street.
- Relocating utilities underground.
- Period street lighting that consist of a single, simple fixture and pole. Refer to the recommended street lighting in the appendix.
- Development of Carriage Town Square.
- Period accessories that include signage, benches, etc.
- Existing sidewalks be retained where possible particularly those with dates. New sidewalks should match size, color and texture of existing walks.
- Landscaping to provide major overhead tree canopy and street edge definition.

• CARRIAGE TOWN SQUARE

Carriage Town Square should be limited in its development so as to remain as flexible as possible. Lawn area with minimal landscaping will accommodate a range of events from blue grass festivals to vehicle displays. Recognition of original site access at the end of Mason is fashioned into a recognizable entry point via a boardwalk, landscaping and

public art such as Durant-Dort in conversation. Landscape screening should occur to hold obscure the working portion of the existing railroad.

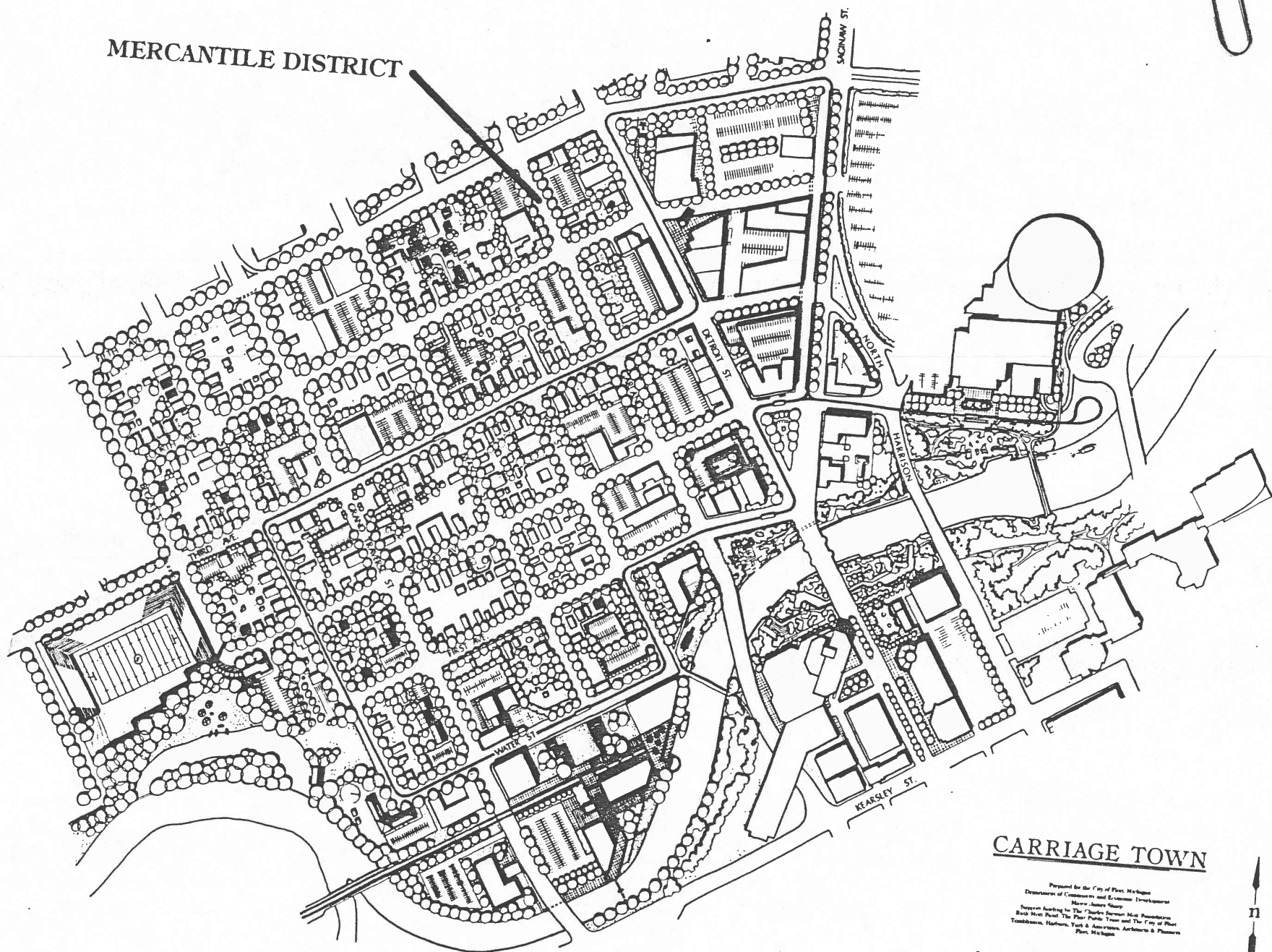
• DURANT-DORT OFFICE PROMENADE

A paved promenade with tree canopy is recommended to display the east facade of the Durant-Dort Building from the intersection of Lyon Street. The promenade area with combination of paving materials, lawn area, iron tree guards and wrought iron fence should be designed to direct attention to the restored office building. Tree rows on the north side of Water Street soften the streetscape between the industrial complex and the residential quarter as they once did at the turn of the century.

MERCANTILE DISTRICT



MERCANTILE DISTRICT

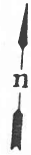


CARRIAGE TOWN

Prepared for the City of Plant, Michigan
 Department of Community and Economic Development
 Mayor James Nease
 Request funding for The Charles Fremont Mass Foundation
 Both Men Pond, The Park Public Trust and The City of Plant
 Tomlinson, Hartman, York & Associates, Architects & Planners
 Plant, Michigan

Carriage Town is a community development project which focuses on historic preservation as a primary component in a comprehensive revitalization program. The preservation of this area has resulted in a new...

The Carriage Town Master Plan is intended to provide the framework for the preservation of the area, to include the preservation of historic structures and the revitalization of the area. Plan's primary role will be to provide public area site improvements if and where funding becomes available.



RESOURCE

. ARCHITECTURAL IDENTITY

A blend of commercial/residential architecture that offers unique potential for community identity with high image ability.

. BUSINESS INTEGRATION WITH NEIGHBORHOOD

Existing business types compliment nearby residential quarters and provide a good transition between the Neon District and Residential Areas.

. EXISTING ADOPTIVE REUSE

Several fine examples of adaptive reuse structures now exist. These residential structures and the character of the streetscape has been preserved through imagination and compatible commercial use.

. MAJOR EAST-WEST ARTERY

Third Avenue, one of two east-west arteries through Carriage Town, creates a high visibility, high image area.

IMAGE DESCRIPTION

A narrow band of business/retail establishments, referred to as the Mercantile District, winds its way through the heart of the residential quarter. The district characterizes a quieter pace of life more intimate and innocence than the flamboyant, energetic development found in the 1920's

hotel and Neon District. The Mercantile District is an old town development, a town of wood frame construction and uneven lines of clapboard siding, sloping roof lines and sundry back-alley sheds.

COMPONENTS

. BUSINESS TYPES

Small speciality shops, professional offices, food markets, pharmacies, restaurants and services that present a neighborhood business charm along the major arteries within the residential quarter provides the essence of the Mercantile District.

. COMPATIBLE ADOPTIVE USE OF EXISTING ARCHITECTURE

Commercial uses of the existing residential structures that can make useful advantage of architecture should be encouraged. Businesses such as dress shops, millinery, gifts, notions, novelties, antiques, boutiques, etc. can effectively utilize the unique, large windows and open porches of these older homes without alterations.

. MODIFICATIONS OF NON-SUPPORTING ARCHITECTURE

The use of fabric awnings, signage, color, lighting and landscape are used to integrate architecturally non-supportive structures into the streetscape and neighborhood environment.

• PUBLIC AREA SITE IMPROVEMENTS

Public area improvements include street resurfacing, landscaping, period lighting and street side accessories.

RECOMMENDATIONS

• ARCHITECTURAL PRESERVATION/RESTORATION

The residential building forms should be preserved and utilized in this commercial area. Restoration should conform to the Secretary of the Interior Standards for restoration and the Design Guidelines of the Flint Historic Commission.

• ZONING CHANGE

Expansion of the D-3 (Community Business District) zoning should not be allowed except where it is herein recommended. A portion of the adjacent D-5 (Metropolitan Commercial-Service District) should be modified to D-3 zoning. This would prevent the permitted manufacturing use that now exists in the D-5 district and would include the areas of the South Grand Traverse, Water Street, a portion of the Third Avenue and Garland Street.

• REDUCE PARKING REQUIREMENTS

Required off-street parking should not jeopardize the character of the Mercantile District by removing buildings to create parking lots. Collective off-street parking

spaces and joint use of facilities need to be encouraged beyond present zoning requirements. Maximum distances between parking and establishments served should be increased to allow for greater use of larger existing lots in the area. Property tax incentives and/or business user fees could provide financial motivation for the owners of these large lots. Reducing the required off-street parking by 25 to 30% and relaxing the off-hour joint use parking requirement will help to create a pedestrian oriented district. Parking areas should not be allowed in the front yards but have a minimum setback along Third Avenue and Grand Traverse of 20 feet or the existing setback of the building which ever is greater. Parking areas along Garland Street should have a minimum setback consisting of 20 feet lawn area with deciduous trees or 7 feet setback with evergreen/ hedge screen plantings. The use of brick screen walls and open wrought iron fencing in combination with minimum landscaping to soften the appearance of the 7 feet setback would also be appropriate.

- Modifying existing architecture which is non-supportive to the desired character of the district. A building facade should not be changed to make it look 'historic' but to better integrate it with the neighborhood. Several key design elements greatly influences the visual quality of the Mercantile's District streetscape: COLOR, PROPORTION, MATERIAL, SIGNAGE AND LANDSCAPING.

- COLOR - bright and flashy day-glow colors should be avoided. Select similar colors to tie the various building components together. Roofs should be dark, rich colors. Architectural features can be highlighted with a slightly different color than the facade color.
- PROPORTION - the relation between width and height of the existing facades should be respected. Proportions of doors and windows are also important. Window and door arrangements should not emphasize the horizontal but emphasize the vertical or the nearly transparent 'storefront'. Roofs should be flat with a parapet or a minimum 6/12 slope.
- MATERIAL - The original, human scaled materials such as common face brick, 4" horizontal wood siding, etc. should be the basic elements. Substitute sidings, metal fascias, plastic roofs or the use of several different facade materials violates the one or more of the key design elements.
- SIGNAGE - Signage should relate to the pedestrian. Large individual letter signs, signs on top of buildings, covering upper facades, flashing, blinking lights or motion signs are not appropriate in the Mercantile District. Internally lighted signs or the use of lighting to create images or letters are not recommended. All signs should be externally lit by inconspicuous fixtures or the spill over light from period

street lighting or buildings. A variety of materials are available and with careful design can be appropriate. Sign placement on the building is also important and should be limited to:

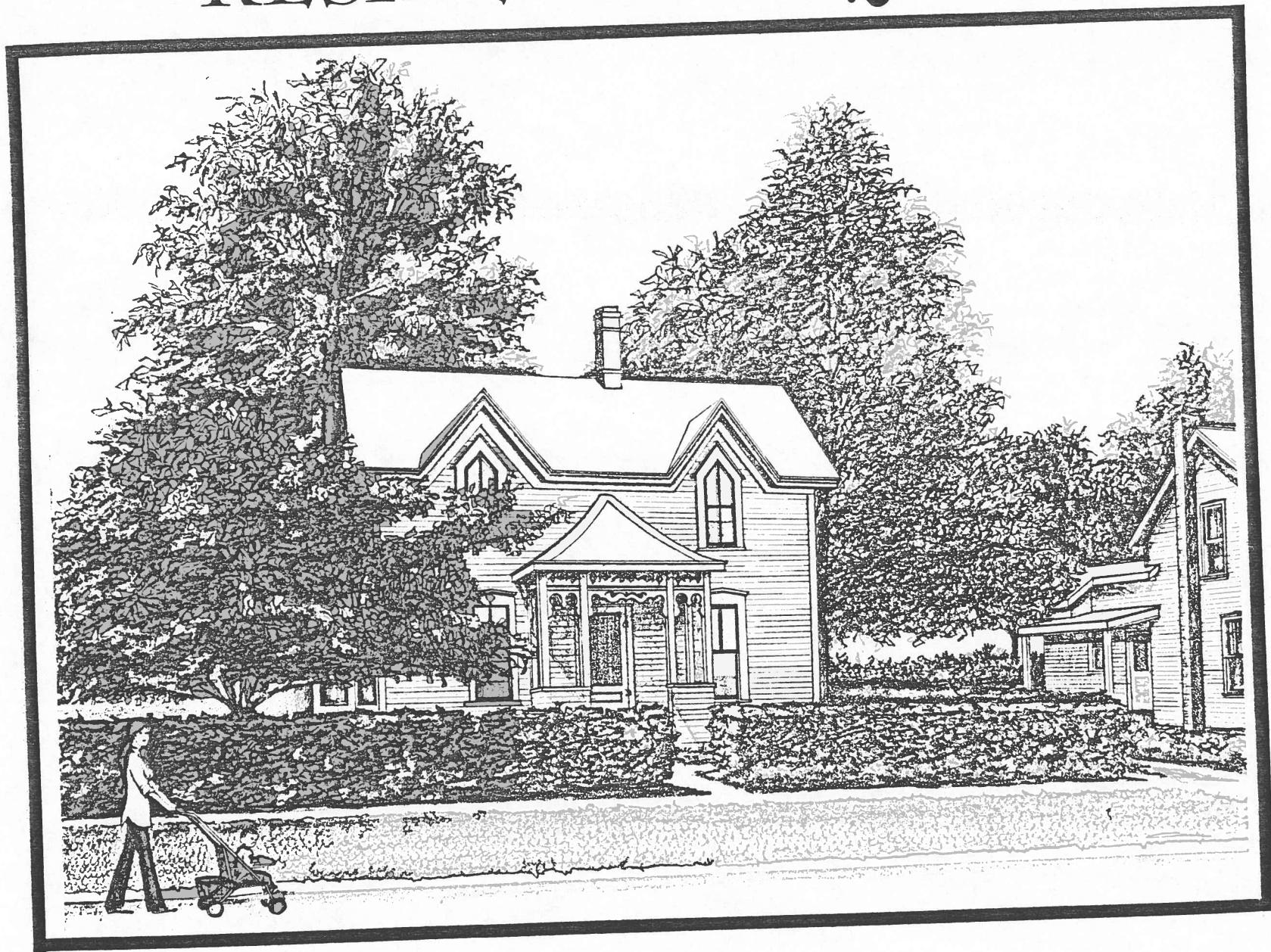
- Under the lower cornice
- On the awning flap
- Hanging from the building
- Painted on the glass

Examples of appropriate signage can be found in the appendix.

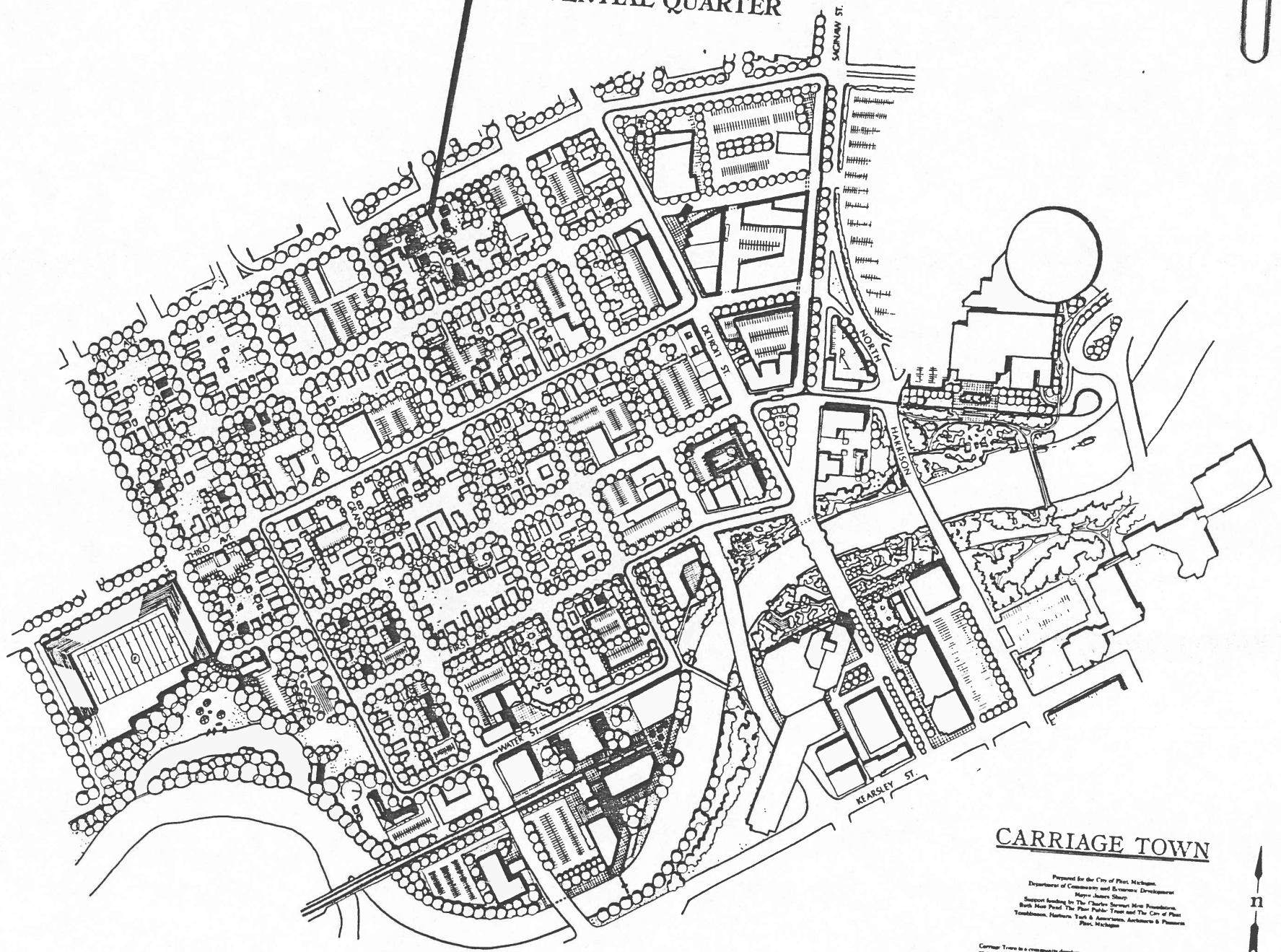
- LANDSCAPING - Private landscaping efforts should provide lawn area, foundation plantings and small decorative trees. Fences should be wood picket, brick or iron (not chain link) and be used sparingly to screen parking lots and bridge the open lot voids in the streetscape.
- PUBLIC AREA SITE IMPROVEMENTS
Public area site improvements should include the following:
 - Brick resurfacing of Third Avenue from Atwood Stadium to Saginaw Street.
 - Relocating utilities underground.
 - Period lighting that consists of a single, simple fixture and pole. Refer to the recommended street lighting in the appendix.

- Consolidating street signage to the same pole. Poles should be solid metal in a very dark finish.
- Period streetscape accessories (benches, litter containers, etc.) where warranted.
- Reviving period signage for street names and information.
- Landscaping to provide major overhead tree canopy.
- The 'non-improvement item' of retaining the original street grid pattern. Reducing curb to curb widths or alteration of straight curb is not an appropriate public improvement. The present street grid has historic presence with its relation to the Indian Reservation survey.

RESIDENTIAL QUARTER



RESIDENTIAL QUARTER

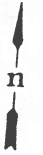


CARRIAGE TOWN

Prepared for the City of Flint, Michigan.
 Department of Community and Economic Development
 Mary-Anne Shroy
 Support funding by The Charles Sargent Howe Foundation,
 Ruth Hill Pease, The Flint Public Trust and The City of Flint
 Tomlinson, Mathers, Turk & Associates, Architects & Planners
 Flint, Michigan

Carriage Town is a community development project which focuses on historic preservation as a prime component in a comprehensive revitalization program. The purpose of this plan is to lead new directions through the preservation and enhancement of the resources of this historically rich area.

The Carriage Town Master Plan is intended to provide the framework for the project area to encourage the preservation of historic structures and the revitalization of the area. No public acquisition of property is anticipated or included in the Master Plan. The City of Flint's primary role will be to provide public area site improvements if and when funding becomes available.



RESOURCE

• RESIDENTS

The residential quarter provides a fundamental ingredient to Carriage Town - people. People at home; working, playing, relaxing. A living residential community developed in the context of Carriage Town renders a sense of realism to Carriage Town as a whole.

• WELL-DEFINED NEIGHBORHOOD

Small, intimate neighborhood boundaries. Physical barriers such as major roads and contrasting land use create four distinct residential quarters in Carriage Town. An area distinction as this can be effective in establishing resident involvement and addressing community needs at a very personal level.

• HISTORIC SIGNIFICANTS

The historic significance of several homes - Nash, Aldrich, Dodds/Dumanois, Summerfield, Randolph/Higgins, Mason, Phillips, etc.

• ARCHITECTURAL RESOURCES

The diversified architecture of the residential structures creates neighborhood character and visual interest.

• PUBLIC AMENITIES

Tree shaded residential streets impart a peaceful setting. Combining with turn of the century architecture, an idealized, romantic setting is created.

IMAGE DESCRIPTION

Quiet, tree lined streets, white picket fences and the intriguing character of clapboard siding houses reflect the 1800's character in the residential quarter of Carriage Town. This present day residential area respects and capitalizes on its resources while respecting the privacy of the inhabitants. It is an area that is inviting and of historic interest to a tourist in a slow, quiet, less congested environment. It is a place of refuge and leisurely strolls for residents and hospice guests.

COMPONENTS

• RESTORED HOUSING STOCK

A restored housing stock can meet several different housing needs. The most neighborhood stability and pride results from owner-occupied dwellings. Rental units, however, can exist within a positive atmosphere. Temporary housing needs for students and tourists can benefit Carriage Town by adding population and movement throughout the day.

• TOURIST HOMES

Bed and breakfast tourist homes can provide low and moderate living accommodations. "Stay in the village during your visit to Carriage Town and AutoWorld" is a promotional boost for the entire area.

- RECEIVING GROUND FOR SPECIAL HOMES SCHEDULED FOR DEMOLITION

Infill housing saved from demolition through renovation and moving to available lots in Carriage Town provides true period housing. The program would achieve several objectives, one it provides cost effective housing; two, it preserves attractive architecture which would be lost and expensive to duplicate; three, it maintains the residential streetscape of Carriage Town with authentic architecture.

- NEIGHBOR PARKS

Small vest-pocket parks that preserve open space throughout the residential quarter. Vacant lots maintained by residents of the immediate area provide safe play areas for children.

- PUBLIC AREA SITE IMPROVEMENTS

Public area site improvements include appropriate street resurfacing, sidewalk repair, period lighting, landscaping, curb and gutter, traffic signage, historic markers and place making public art.

RECOMMENDATIONS

- ARCHITECTURAL PRESERVATION/RESTORATION

Housing restoration should conform to the Secretary of the Interior's standards for restoration and the design guidelines of the Flint Historic Commission. The fundamental

intent of these standards are to maintain, recover and/or restore the original building form, proportions, materials and details. The recovery and restoration of these items are of primary importance in conveying the character of the late 1800's.

- NEW CONSTRUCTION

Several of the newer apartment buildings have demonstrated sensitive to the neighborhood character. New building construction needs to respect the relation of scale, proportions, setback, materials, mass and form with the older homes and should only be encouraged where non-historic homes and unrehabilitable housing exists. The design of the new construction should not overpower the existing architecture, but serve as a complement to.

- DIVERSITY OF HOUSING ACCOMMODATIONS

Encourage a diversity of housing accommodations for continuing residents, interim tenants and tourists. Incentives such as local property tax abatements and freeze at pre-restoration levels will encourage restoration compliance with owner-occupied houses. Income-producing structures such as apartments and tourist homes currently have a 25% federal tax credit incentive available.

- HOUSING RELOCATION/RESTORATION PROGRAM

An alternative to new construction which shouldn't be overlooked is moving houses scheduled for demolition to Carriage Town and restoring them. Selective period houses built prior to 1915 could gradually fill

vacant lots. The relocation/restoration program could be similar to the Battelle in Columbus, Ohio and be administered by existing agencies such as Flint NIPP. Such a program would in many ways be a more desirable approach than new construction.

• VEST-POCKET PARKS

The preservation of open space within each of the distinct neighborhoods comprise the Residential Quarter. Distinction for the open spaces derives from the natural and urban influences of each specific setting. However, general considerations should provide for young childrens' (under 8) play area and an adult area for passive relaxation. Ideally located at or near the geographic center of the neighborhoods and visible, such as a corner lot, these vest-pocket parks would be family oriented. Improvements, limited to sand boxes, swings, etc. and maintained/policed by residents would deter undesirables.

• PUBLIC AREA SITE IMPROVEMENTS

Improvements should include the following:

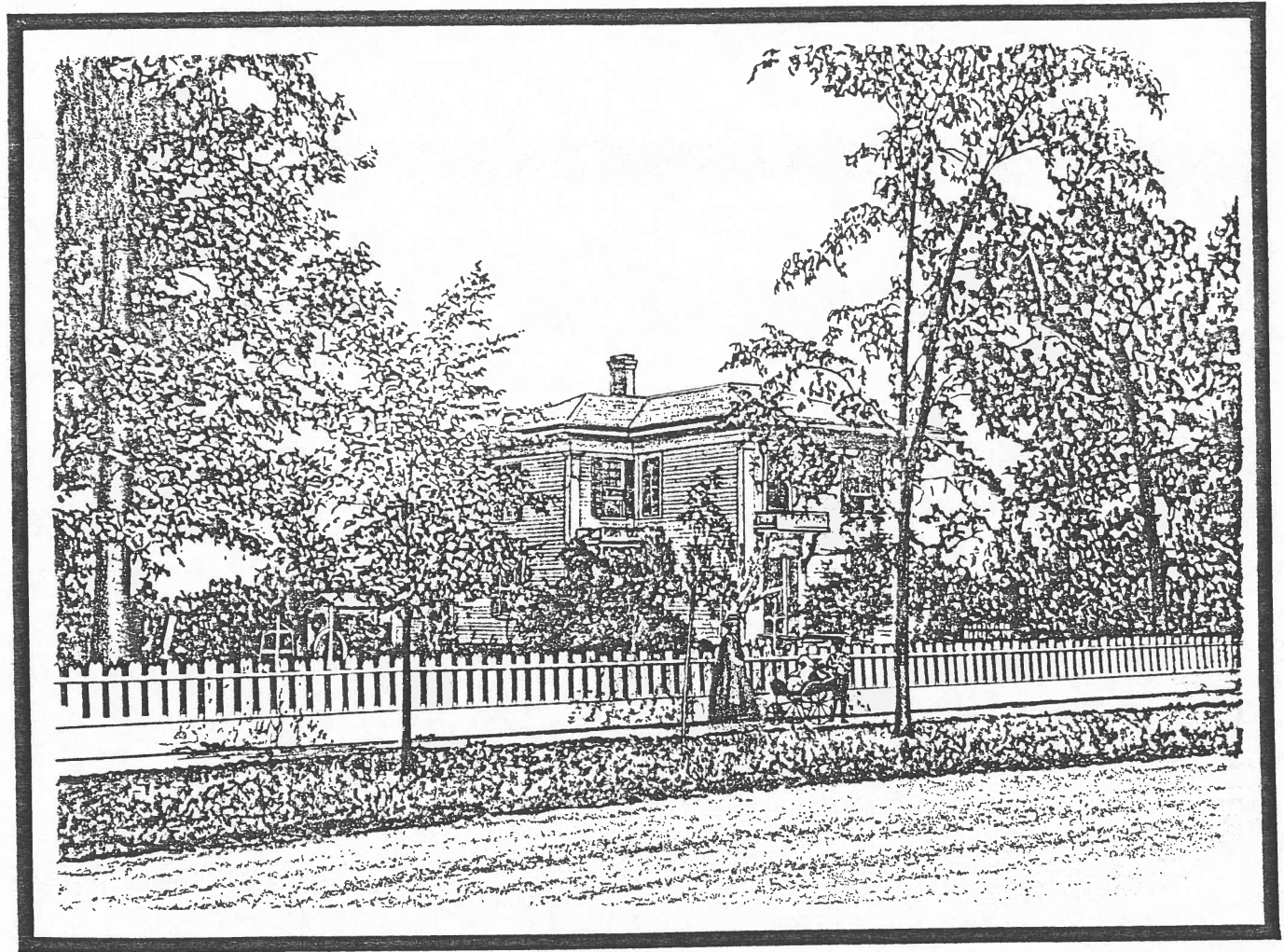
- Street resurfacing with bricks is recommended. Paving alternatives for brick could include an exposed aggregate concrete or asphalt within the residential areas only. Such a plan would allow distinction between the major streets/tourist areas and the residential quarters.

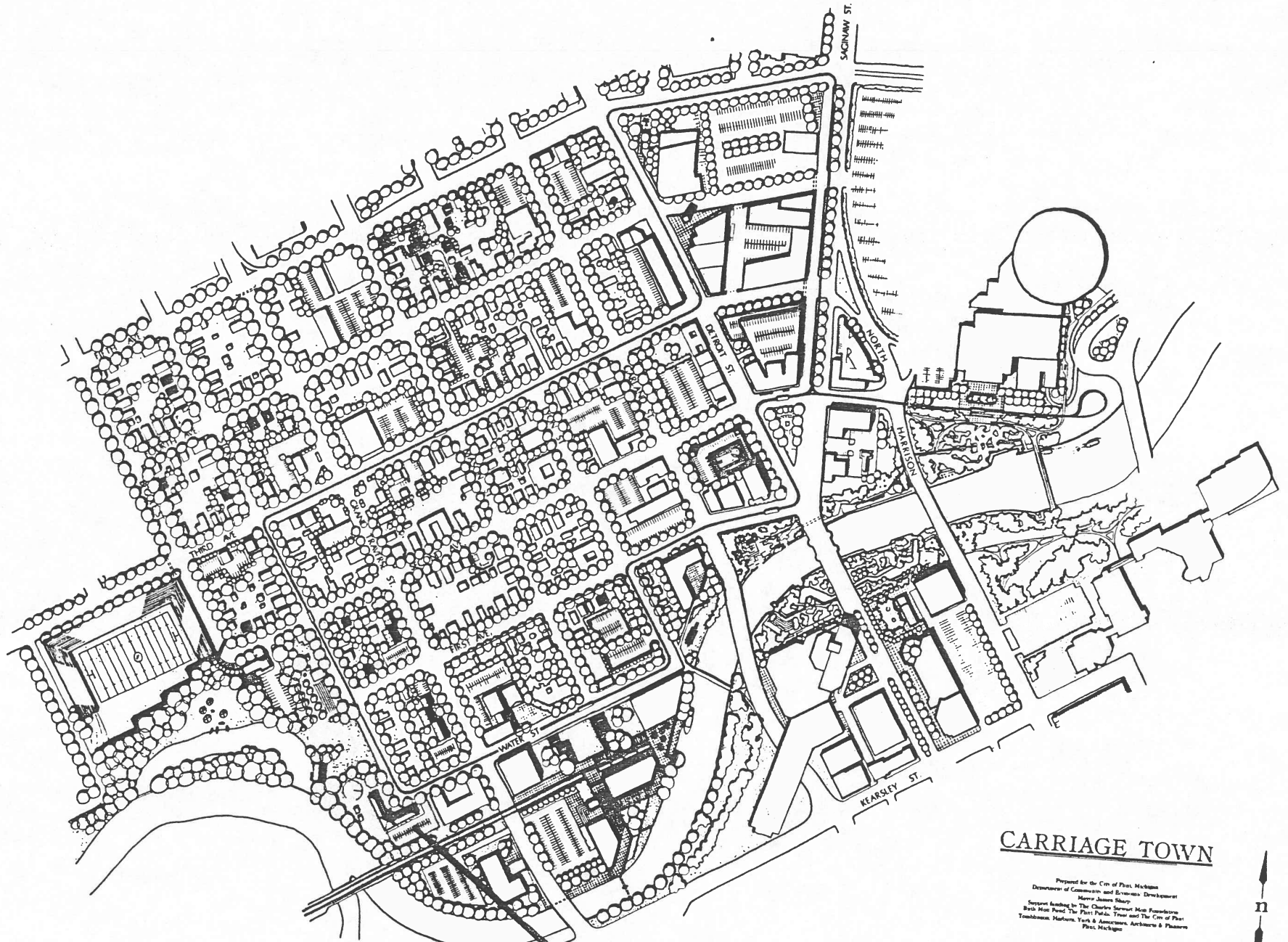
- Concrete sidewalk replacement should match the color and texture of the older existing walks. Existing walks, particularly those with early 1900 dates, should be preserved and reused.
- Street lighting should be a single pole, single fixture, the same as recommended in Carriage Factory District. Refer to the recommended street lighting in the appendix.
- Curbs should be the straight (period type) curb and not the more recent roll curb.
- The gradual relocation of utilities underground.
- The secure placement of distinctive street signage that was first used in Flint. Unsubstantiated evidence suggests a white letter on black or blue background.
- Landscaping should be limited to major overhead canopy plantings that reinforce the existing trees. Shared responsibility between city and property owner for replacing r.o.w. trees lost to disease or nature will maintain the neighborhood character.
- Private landscaping efforts should encourage color and year round foliage with the use of plant materials outlined in the appendix.

. PUBLIC ART/INFORMATION

Historic markers, walking tour markers, paving inserts, information kiosk and realistic sculpture depicting personages or events are recommended to provide public awareness and appreciation of the area.

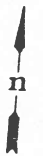
URBAN FARM AREA





CARRIAGE TOWN

Prepared for the City of Flint, Michigan
 Department of Community and Economic Development
 Mayor James Skary
 Technical leading by The Charter School Music Foundation
 Built with Funding from The Flint Public Trust and The City of Flint
 Tomlinson, Hartman, York & Associates, Architects & Planners
 Flint, Michigan



URBAN FARM AREA

Carriage Town is a residential development project which focuses on historic preservation as a principal criterion in a comprehensive revitalization program. The purpose of this plan is to help the area develop through the preservation and enhancement of the structure of the historically rich area.
 The Carriage Town Master Plan is intended to provide the framework for the development and the preservation of historic structures.

RESOURCE

• EARLY FLINT HISTORY

Flint's early history, a microcosm of urban development for most northern U.S. cities, is a recognized part of Carriage Town. Flint's heritage illustrates the continuum of human growth and development.

• WESTERN TERMINUS

The opportunity to create a meaningful western terminus of Riverfront Park.

IMAGE DESCRIPTION

A quiet meadow, surrounded by stands of white pine and hardwood occurs at the western edge of Carriage Town, near the bend in the river. The setting is reminiscent of the way it could have looked in the mid 1800's. The meadow on the north changes to orchard and gardens. Beyond the gardens a restored house bustles with activity from the 'Urban Farm'. Further west just as the meadow land falls to the river a contemporary skeleton frame, symbolic of the lumbering era sawmill, raises over the hill. From this meadow, near the grand traverse of the Flint River, the commemorative park extends the pioneer in influences throughout the city.

COMPONENTS

• OPEN SPACE

A semi-natural open space setting is created to provide a backdrop of evergreen and deciduous trees. This park provides a quiet, picturesque picnic/passive activity area overlooking the Indian Village and establishes an early urban farm ambiance.

• HORSE STABLES

Functioning horse stables serve Carriage Town's horse drawn taxis, city support vehicles and the mounted police patrol. A common facility, zoned separately for the various users, provides cost effective stables necessary for a 'total environmental plan' in Carriage Town. Horses for city use could include functions such as street cleaning as well as mounted police patrols in Carriage Town. Even a small foot patrol or precinct station could be developed with the mounted police patrol facilities.

• LUMBER MILL SCULPTURE

Flint's first major industry, lumbering, is celebrated with a ghost image sculpture of a sawmill, perhaps the Atwood Sawmill, located at rivers edge. The sculpture, similar in concept to Ben Franklin's house in Philadelphia, Pennsylvania, could be functional to the extent of its waterwheel pumping river water through the Indian Village. Markers, identifying the various lumber mill sites in Flint, can also serve to direct people to the sculpture.

- URBAN FARM

The 'Urban Farm' joins the efforts of the F.E.A.T. (Flint Environmental Action Team) Foundation and the Carriage Town master plan. A restored house on the edge of the open space serves as F.E.A.T.'s headquarters. Administrative functions, meeting rooms, garden permit processing and recycling plant can operate from this headquarters. Surrounding land area could be used for organic gardening convenient to the horse stables, research/demonstration gardening and nursery stock for later replanting throughout Flint.

RECOMMENDATIONS

- WESTERN TERMINUS TO RIVERFRONT PARK

The creation of a definite western terminus of Riverfront Park is recommended. A semi-natural setting is suggested to minimize maintenance and lend to the overall character of the Urban Farm area. Planting of both evergreen and deciduous trees is necessary at strategic locations to screen the factories and warehouses to the south and west. The development should also provide the necessary amenities to support a quiet riverside picnic area.

- JOINT-USE DEVELOPMENT OF HORSE STABLES

Encourage the construction of a horse stable in the area. The stable could possibly be developed as a joint effort between the public and private sectors. It could serve as headquarters for the Carriage Town taxi service, the mounted police patrol, the Michigan Horse Drawn Vehicle Association and the Michigan Morgan Horse Association.

- DEVELOPMENT OF THE URBAN FARM PROGRAM/FACILITIES

Encourage the development of the 'Urban Farm' concept and related F.E.A.T. activities within this area.

- RECOGNITION OF THE LUMBERING ERA

Recognition of Flint's lumbering industry should be undertaken through the use of visual aids. On site, a ghost skeleton frame of a sawmill could also be functional with the waterwheel pumping water to the Indian Village. All known lumber sites in the city should be marked with a brief history and other sites noted.

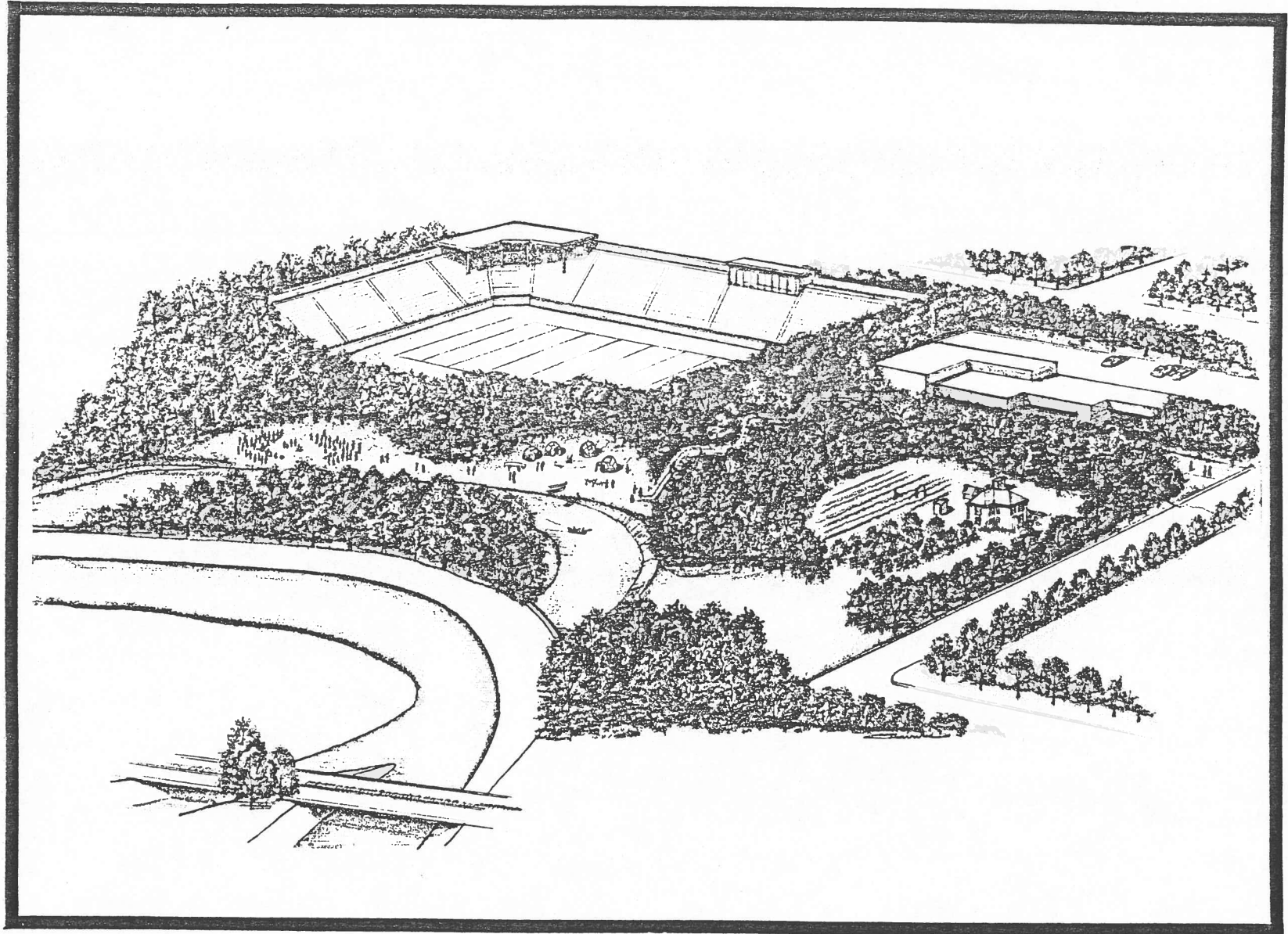
- PHYSICAL INTERPRETATION OF 1819 TREATY SURVEY

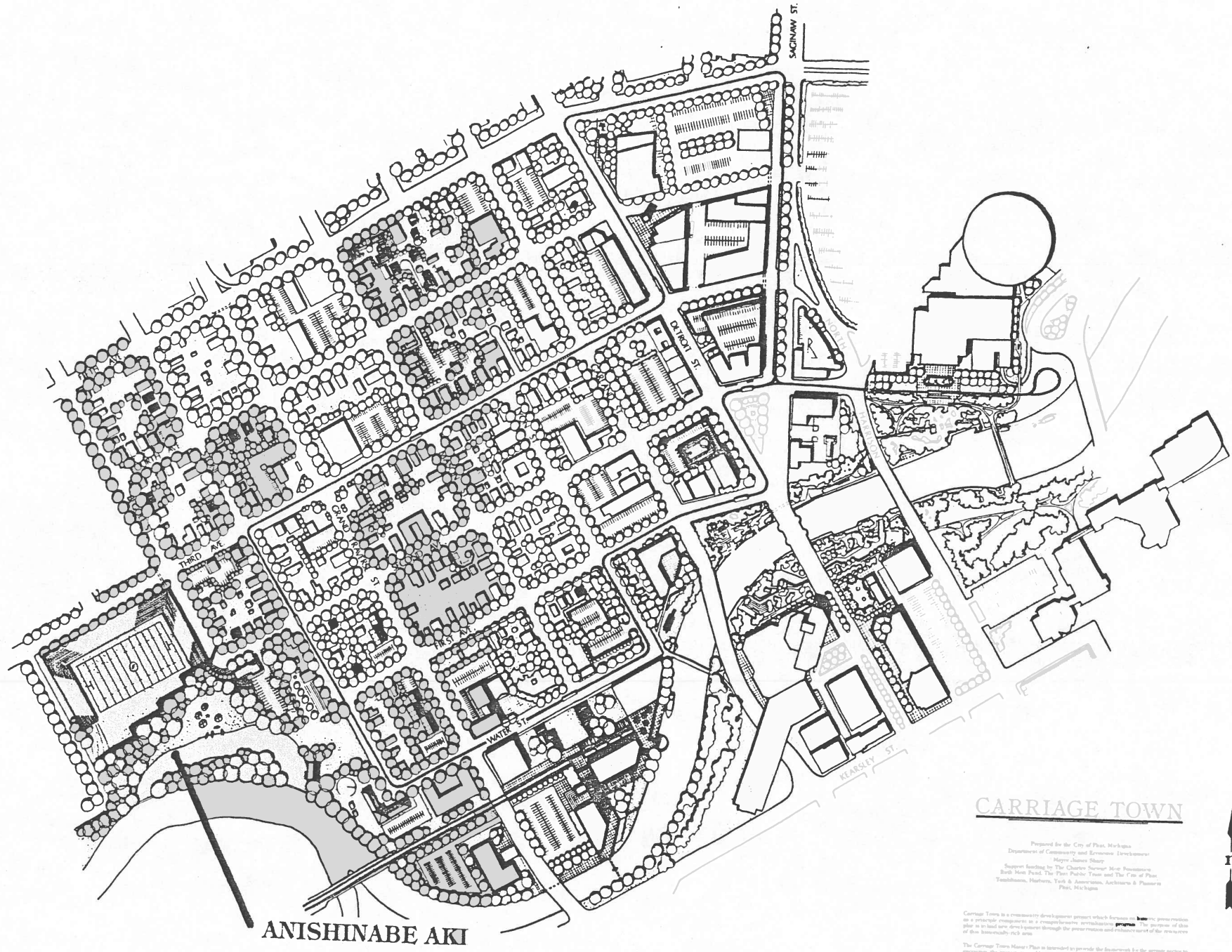
The physical location of the original Indian Reservation plat lines can be incorporated into a public art plan. Local surveyors could be encouraged to physically locate these lines. The patterns of urban development, as they were influenced by these plats that were laid out in 1819, can be seen with respect to our streets, building lines and man made development today.

• PUBLIC ART

The development of public art which would locate the sites of Jacob Smith's trading post, Todd's Tavern, Stone Woolen Mills, etc. is recommended. This would extend AutoWorld to Flint today and add historic fact to the Saginaw Street under the dome.

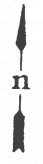
ANISHINABE AKI





CARRIAGE TOWN

Prepared for the City of Plain, Michigan
Department of Community and Economic Development
Helen James Shaw
Superior Planning by The Charles Swaney Most Associates
Bank Street Park, The Plain Public Trust and The City of Plain
Tombstone, Hutton, York & Associates, Architects & Planners
Plain, Michigan



ANISHINABE AKI

Carriage Town is a community development project which focuses on historic preservation as a principal objective in a comprehensive revitalization program. The purpose of this plan is to lead and direct a course through the preservation and historic use of the resources of this historically rich area.
The Carriage Town Master Plan is intended to provide the framework for the private sector to regenerate the historic resources and the revitalization of the area.
No public requirements of procedures as anticipated or required by the Master Plan. The City of Plain's primary goal will be to provide public uses and improvements if and when funding becomes available.

RESOURCE

• HISTORICAL SIGNIFICANCE OF THE AREA

The historic significance of the area as an early Indian gathering place and grand traverse of the Flint River. Possibly the site of one of the earliest North American Indian trails. Historically known as the location of the Smith Reservation and a point of early commerce.

• HIGH CONCENTRATION OF INDIAN POPULATION

The highest concentration of Indians in Genesee County is in the area of Carriage Town between the Flint River and Hurley Hospital.

• POTENTIAL DRAW

The development of Anishinabe Aki has potential to attract native Americans and non-Indians.

IMAGE DESCRIPTION

Anishinabe Aki is a museum where the present day Indian Community presents their traditions, customs and laws that guided the lives of their ancestors who lived along the Flint River over 200 years ago. Anishinabe Aki can be a mecca for the Indian communities of the midwest region in addition to the 40,000 Indians of Michigan. It becomes a major attraction for visitors and 'western anchor' in Carriage Town complimenting the 'eastern anchor' AutoWorld.

COMPONENTS

• INDIAN CENTER

An Indian center is used to acquaint visitors to this region's three major tribes, Ottawa, Chippewa and Potawatomi. Inside, the center history is explained from the Indians' point of view through exhibits, artifacts, oral history and films.

• RESTAURANT

A restaurant within the complex which serves authentic Indian dishes such as buffalo, wild rice, fry bread, corn, potato soup and maple sugar. A unique dining experience awaits the customer in colorful, traditional Indian surroundings uniting the interior with the outdoors.

• RETAIL STORE

A retail store that sells local Indian arts and crafts. Retail sales combine with Indian art seminars, demonstration and eminent American Native artists' exhibits that attract broad base support.

• EDUCATIONAL FACILITIES

An educational institute and administrative offices which serve the Indian community. From here, educational and cultural awareness studies assist the local Indian community in developing personal qualifications and self identity.

- INDIAN VILLAGE

An Indian village functioning within the living museum concept where local Indians work and instruct their children in a realistic setting doing the everyday tasks of their forefathers. Wigwam construction, birch bark canoe building, wild rice gathering, maple sugar harvesting, cooking and dancing are presented out-of-doors year round.

RECOMMENDATIONS

- LOCATION OF ANISHINABE AKI

To maintain the basic design concept, being the progression of history, and to establish a substantial yet factual development, it is recommended Anishinabe Aki be developed at the far western edge of Carriage Town.

- DEVELOPMENT CONCEPT

Anishinabe Aki should be developed first and foremost as a place where area Indians and visiting Indians will feel comfortable to gather. Secondly, it should be developed as an educational center for Indian culture and adult job training/education. Thirdly, it would be an attraction for non-Indian visitors.

- RIVER RELATED

Anishinabe Aki should be river related, if only a symbolic fashion. The conceptual master plan suggests the creation of a symbolic river adjacent to the Flint river. This river would be at

a higher elevation than the actual river and would not interfere with flood control. Water could be diverted from the Flint river, channeled through the Indian Village and returned to the river as it is done in Riverfront Park. The south bank of this new river also provides an opportunity to develop a landscape buffer between Anishinabe Aki and the industrial/warehouse district to the south.

- LANDSCAPING MASTER PLAN

A careful balance needs to be obtained between landscape screening and vistas. Part of the initial attraction for a non-Indian visitor will be the sight of color and activity within the Indian village. Strategically placed views into Anishinabe Aki from surrounding tourist routes will enhance and maximize the visual allurements.

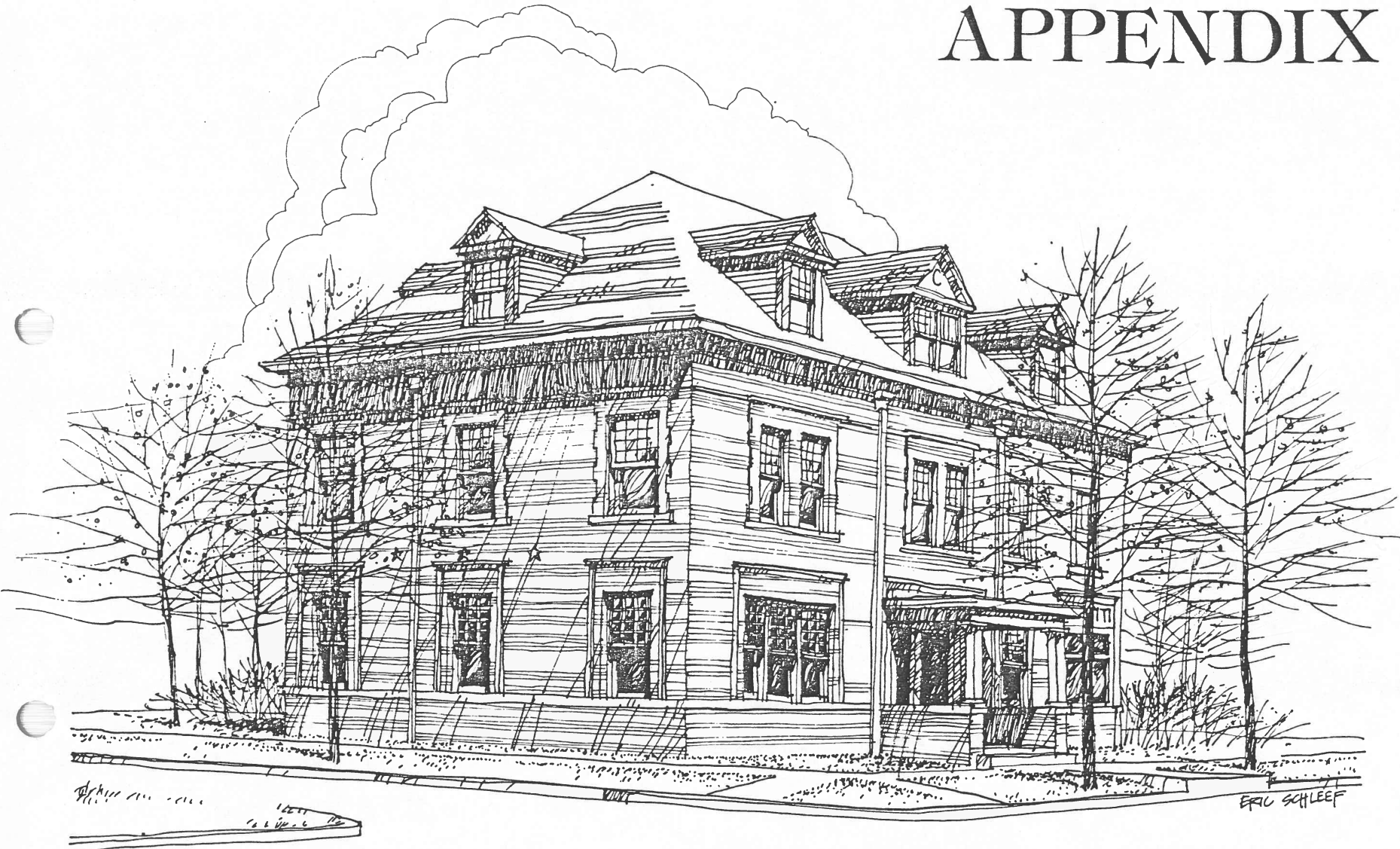
- PROGRAM ATTRACTIONS

Events and activities are necessary that provide both sight and sound. Seasonal festivals, harvest events, pow-wows, etc. with character dress, dancing and music can be the type of activities that attract non-Indian visitors.

- USE OF ATWOOD STADIUM

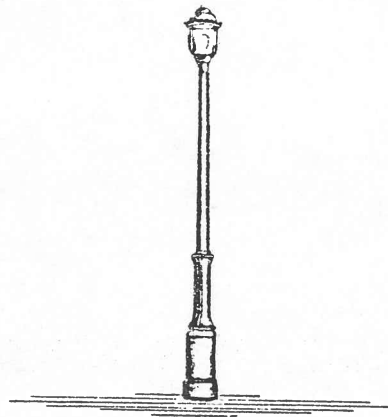
Atwood Stadium should be convenient and made available to the Indian Association for year-round events and demonstrations. Athletic events of Indian heritage, such as lacrosse and snow shoe tournaments, could effectively expand the facilities at Anishinabe Aki without initial capital outlay. The city in turn would benefit by better utilization of an under-used facility and could return the income generated back into maintaining the stadium.

APPENDIX

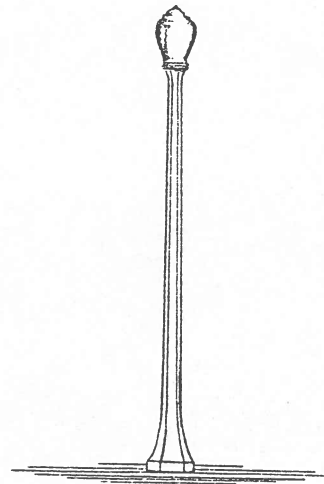


Sketch of house
1/20/11
Eric Schlee

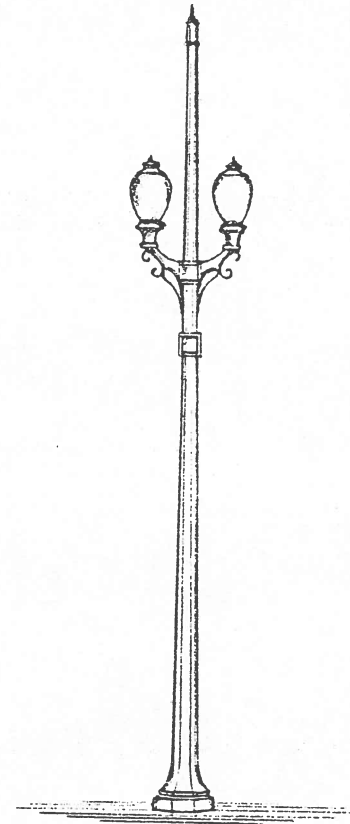
ERIC SCHLEEF



CARRIAGE FACTORY DISTRICT
MERCANTILE DISTRICT
RESIDENTIAL QUARTER
URBAN FARM AREA



NEON DISTRICT
IMA PLAZA

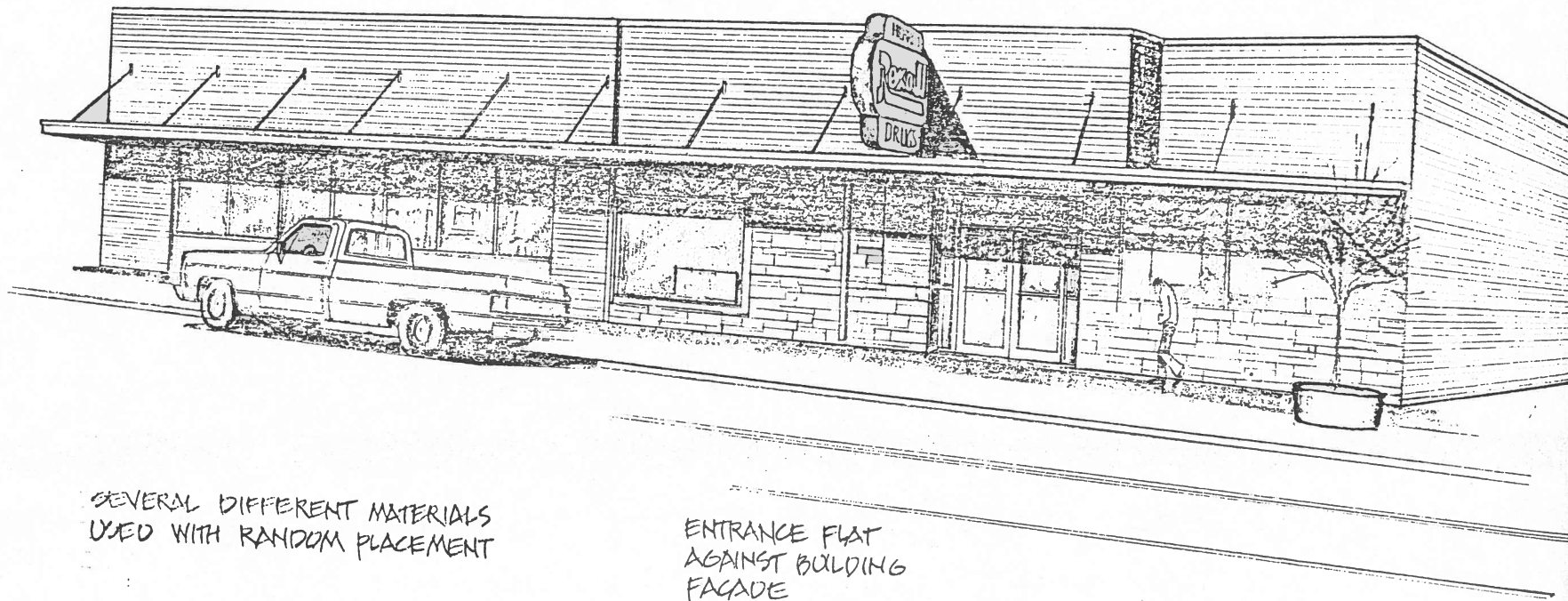


NEON DISTRICT
- SAGINAW STREET
- PORTIONS OF DETROIT

ARCHITECTURAL PROTOTYPE
TYPICAL EXISTING - (NEON DISTRICT)

CANOPY EMPHASIS ON
THE HORIZONTAL

WINDOW PROPORTIONS EMPHASIS
ON HORIZONTAL - DIFFERENT
SILL HEIGHTS



SEVERAL DIFFERENT MATERIALS
USED WITH RANDOM PLACEMENT

ENTRANCE FLAT
AGAINST BUILDING
FACADE

SUGGESTED ARCHITECTURAL MODIFICATIONS

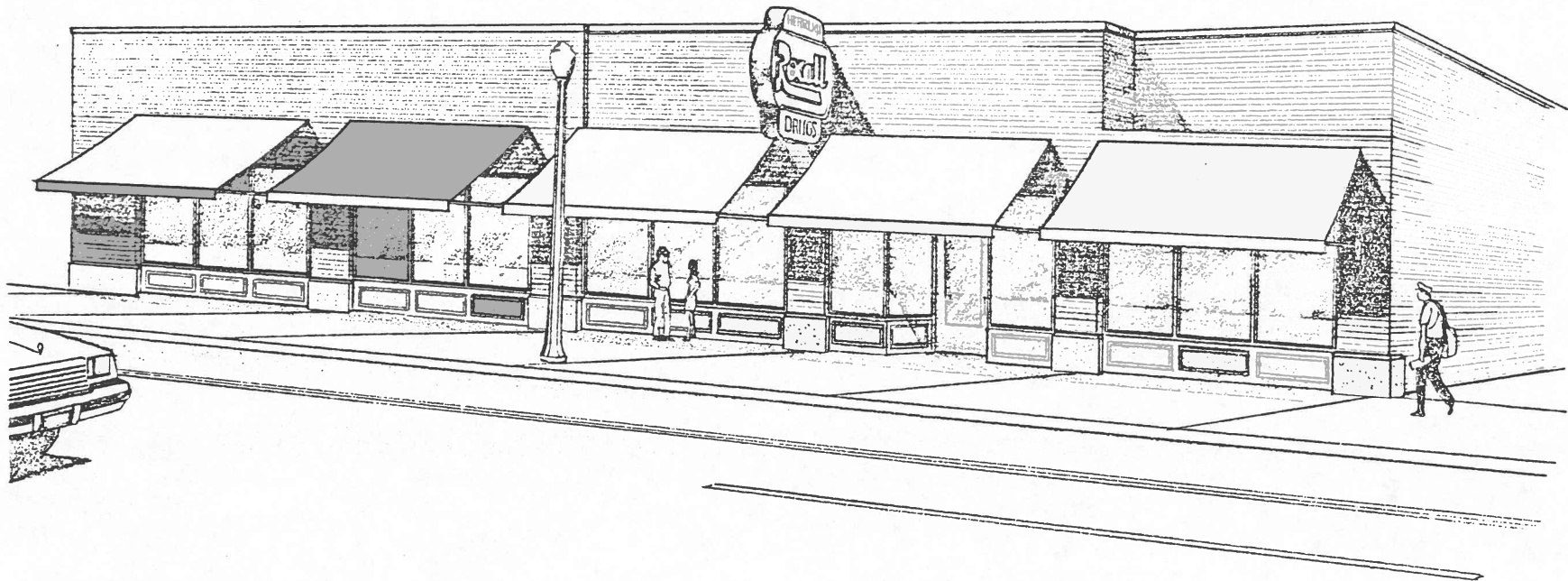
TYPICAL - NEOH DISTRICT

WINDOW OPENING ALTERED
(NOT STRUCTURALLY) TO
SUGGEST INDIVIDUAL
STOREFRONT BAY & LESS
EMPHASIS ON THE
HORIZONTAL

UNIFORM SILL HEIGHT
APPROXIMATELY 2'-0"
ABOVE GRADE

AWNINGS USED TO CREATE
UNIFORM APPEARANCE ALONG
STREETSCAPE & PROVIDE
PROTECTION FOR PEDESTRIANS

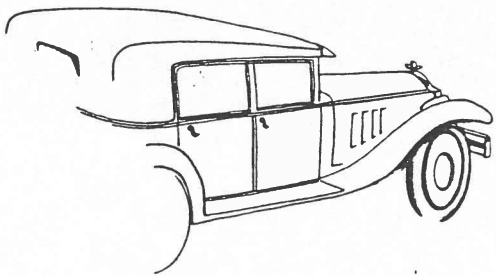
EXISTING SIGNAGE
APPROPRIATE



UNIFIED MATERIALS OCCURRING
IN CONSISTENT PATTERN - MATERIALS
APPEAR SOLID & DURABLE
(BRICK, CUT LIMESTONE ETC.)

ENTRANCE MODIFIED TO CREATE
A RECESS ENTRANCE, DOOR(S)
WITH LARGE GLASS AREA & HIGH
KICK PLATE

ARCHITECTURAL RESTORATION
BUICK FACTORY OUTLET - NEOH DISTRICT



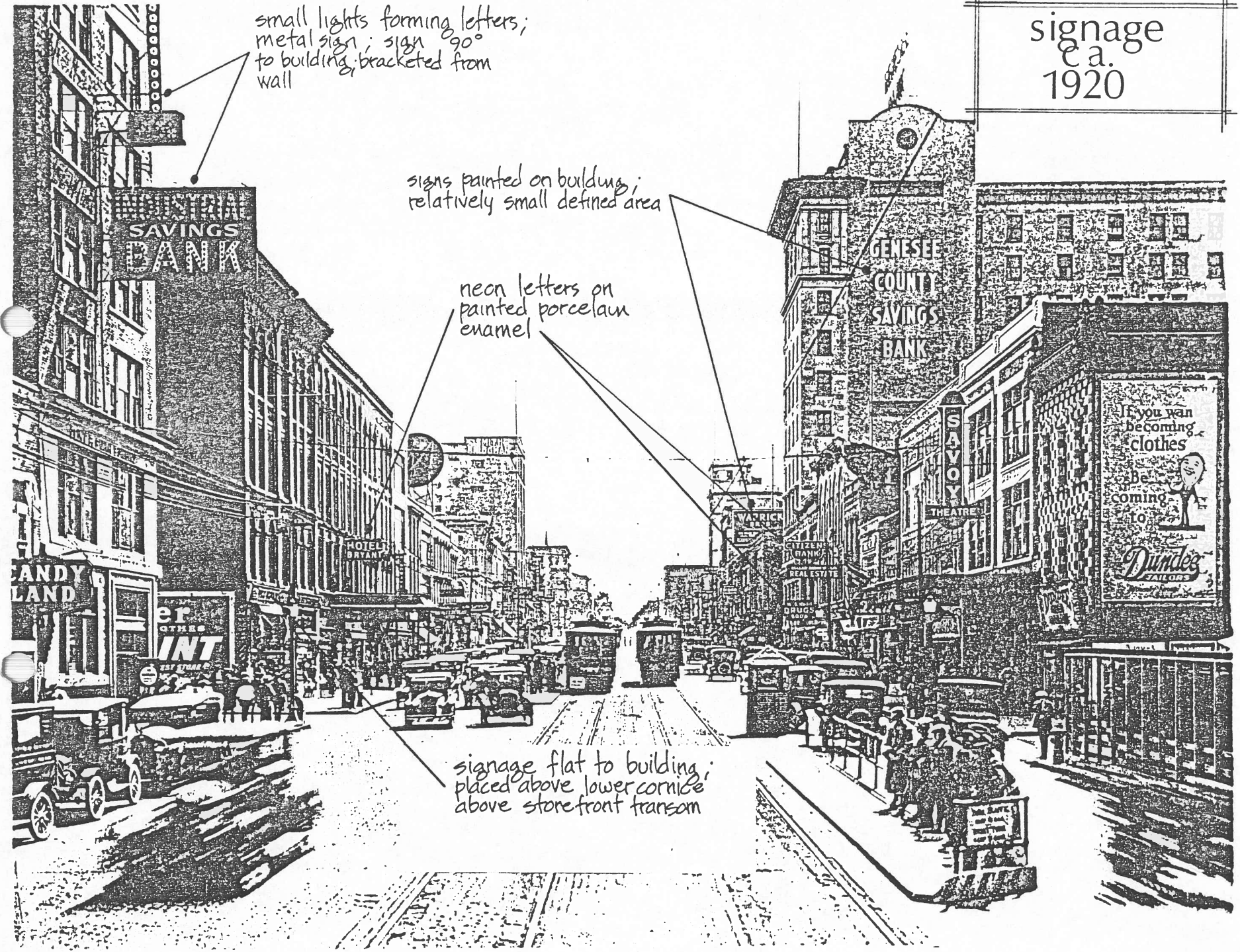
signage
c.a.
1920

small lights forming letters;
metal sign; sign 90°
to building; bracketed from
wall

signs painted on building;
relatively small defined area

neon letters on
painted porcelain
enamel

signage flat to building;
placed above lower cornice;
above storefront transom



GARAMOND ITALIC

A B B C C D D E E F F G G
H H I I J J K K L L M M N N O O
P P Q Q Q Q R R S T T U U
V V W W X X Y Y Z z abcdee fghijk
klmm nopqqrstuvvwxyz as
ctet frgyijisllnt sp tttus 12345
67890 & ? ! B £ \$ (;) ~ ¨ ¨ ¨

A B C D E F G H I J K
L M N O P Q R S T U
V W X Y Z a b c d e f
g h i j k l m n o p q r s t
u v w x y z 1 2 3 4 5 6 7
8 9 0 & ? ! B £ \$ (;) ~ ¨ ¨ ¨

LECTURA BOLD

A B C D E F G H I J K
L M N O P Q R S T U
V W X Y Z a b c d e f
g h i j k l m n o p q r s t u
v w x y z 1 2 3 4 5 6 7 8 9
0 & ? ! B £ \$ (;) ~ ¨ ¨ ¨

GOUDY EXTRA BOLD

A B C D E F G H I J K L M
N O P Q R S T U V W X
Y Z a b c d e f g h i j k l m
n o p q r s t u v w x y z 1 2
3 4 5 6 7 8 9 0 & ? ! B £
\$ (;) ~ ¨ ¨ ¨

FRANKLIN GOTHIC

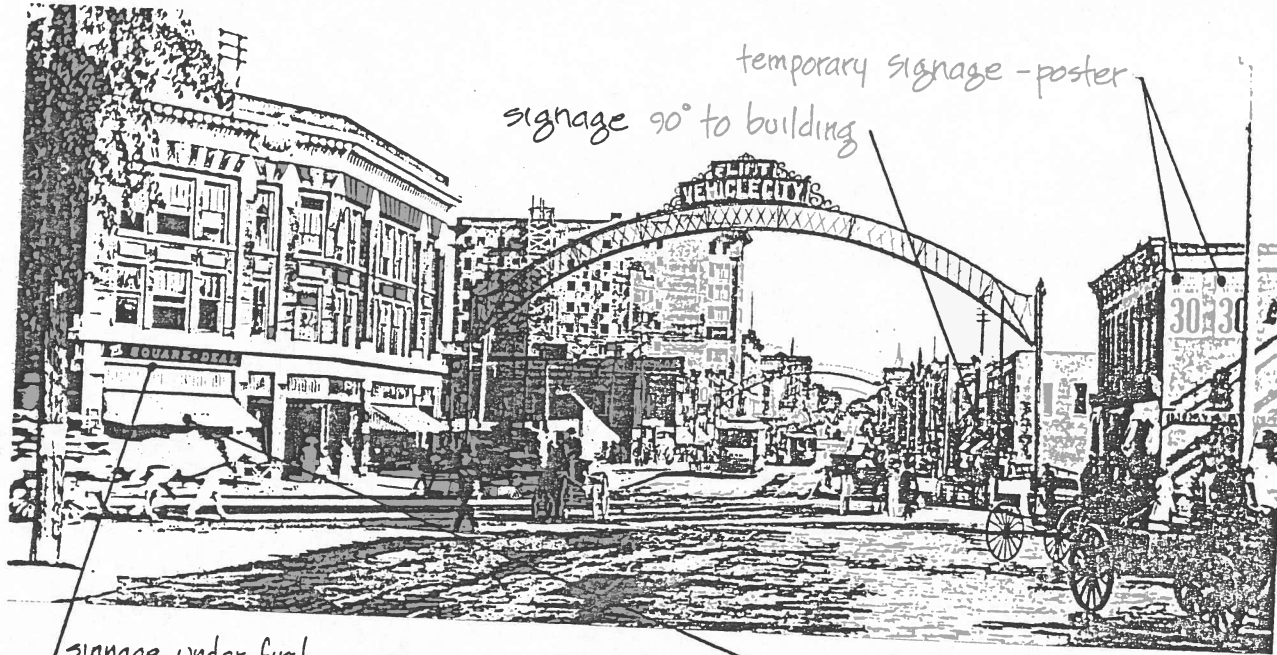
ANTIQUUE OLIVE
BOLD CONDENSED

A B C D E F G H I J K L
O P Q R S T U V W X Y
b c d e f g h i j k l m n
q r s t u v w x y z 1 2 3
6 7 8 9 0 & ? ! B £ \$ (;) ~ ¨ ¨ ¨

A B C D E F G H I J K
M N O P Q R S T U
W X Y Z a b c d e
h i j k l m n o p q r s t
w x y z 1 2 3 4 5 6 7
0 & ? ! B £ \$ (;) ~ ¨ ¨ ¨

OPTIMA BOLD

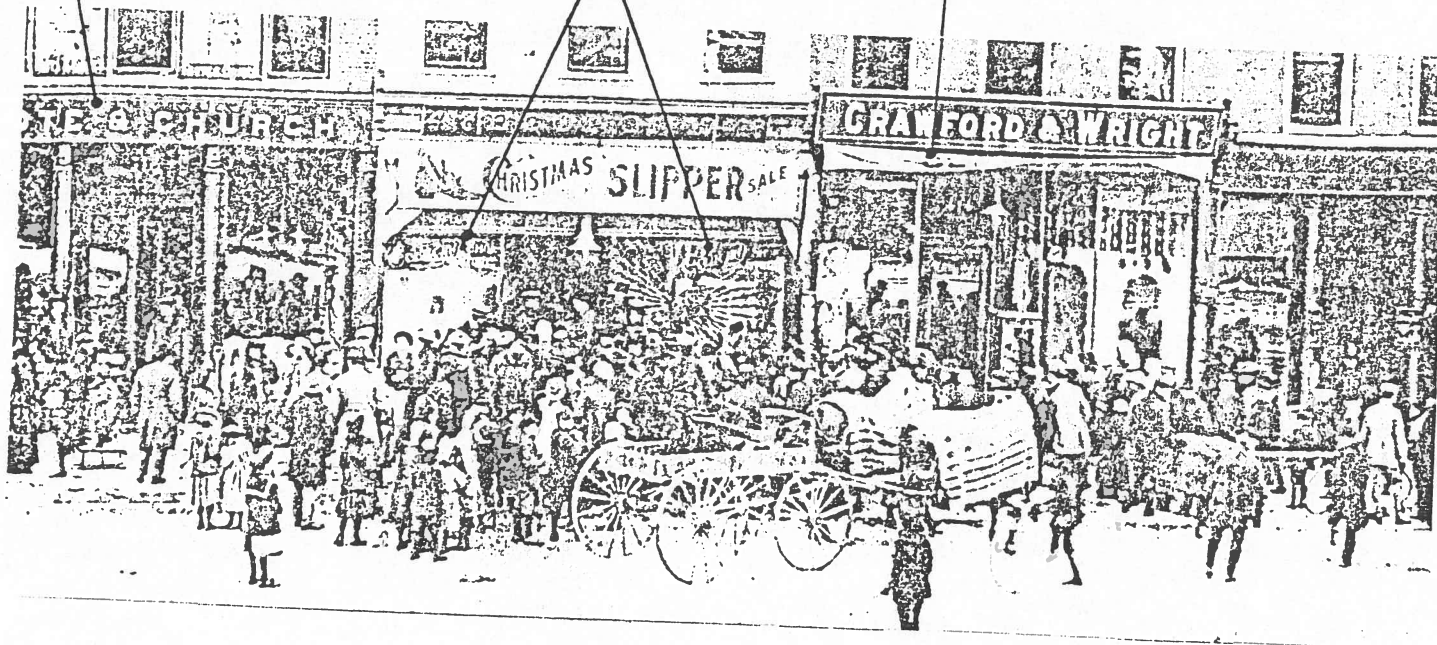
signage ca. 1890



signage under first floor cornice; flat to building; full storefront bay width

signage painted on window

signage on a fabric awning



ABCDEFGHIJ
KLMNOPQR
STUVWXYZ
abcdeefghijkl
mnopqrstuvwxyz
1234567890
&?!B£\$()»»»

TIFFANY MEDIUM

ABCDEFGHIJKLMN
OPQRSTUVWXYZ a
bcdefghijklmnop
qrstuvwxyz12345
67890&?!B£\$()»»»

ANTIQUÉ OLIVE
BOLD CONDENSED

ABCDEFGHIJK
LMNOPQRS
TUVWXYZ
34567890
!£\$()»»»

QUENTIN

ABCDEFGHIJ
KLMNOPQRS
TUVWXYZab
cdefghijklmnopq
rstuvwxyz 12345
67890&?!B£\$()»»»

BASKERVILLE OLD FACE

ABCDEFGHIJ
KLMNOPQRS
TUVWXYZab
cdefghijklmno
pqrstuvwxyz 1
234567890&?!
B£\$()»»»

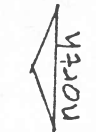
WINDSOR BOLD

ABCDEFGHIJK
NOPQRSTUVWXYZ
Zabcdefghijklmnopqrstuvwxyz
1234567890&?!B£\$()»»»

WINDSOR ELONGATE

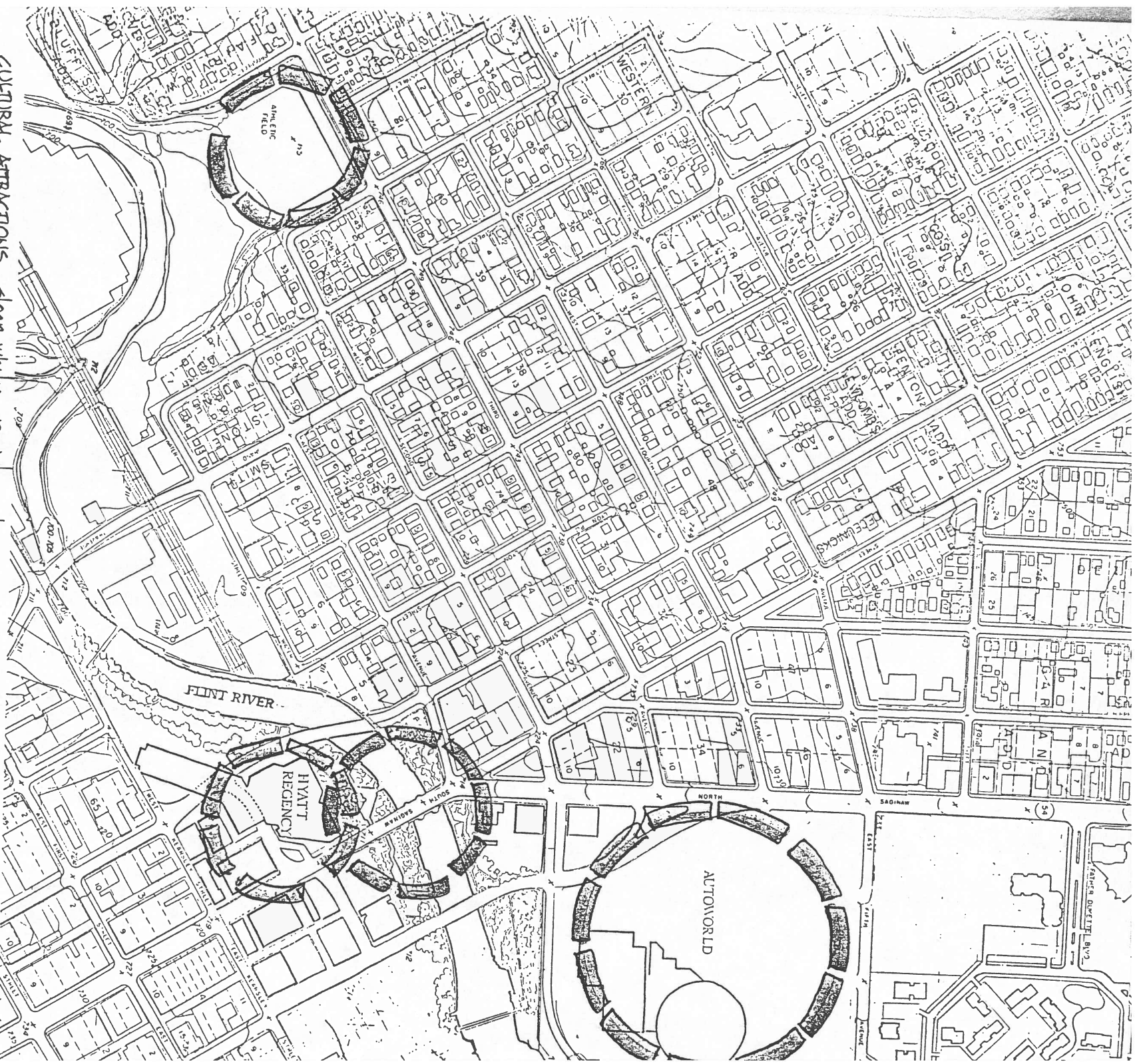


- PRESERVE FORMAL GRID PATTERN OF THE STREETS
- STRONG PATTERN WEST OF DETROIT ST. & PARALLEL WITH DETROIT ST.
 - DEPARTURE AT RIVER / BETWEEN DET. & SAS. ST.
 - PHYSICAL EVIDENCE OF FLINTS GROWTH & THE GEOGRAPHY, ECONOMIC & SOCIAL FORCES



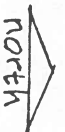
CARRIAGE TOWN
 VEHICLE CIRCULATION .
 GEOMETRIC FORM

Tomblinson, Harburn, York & Associates / townscape inst

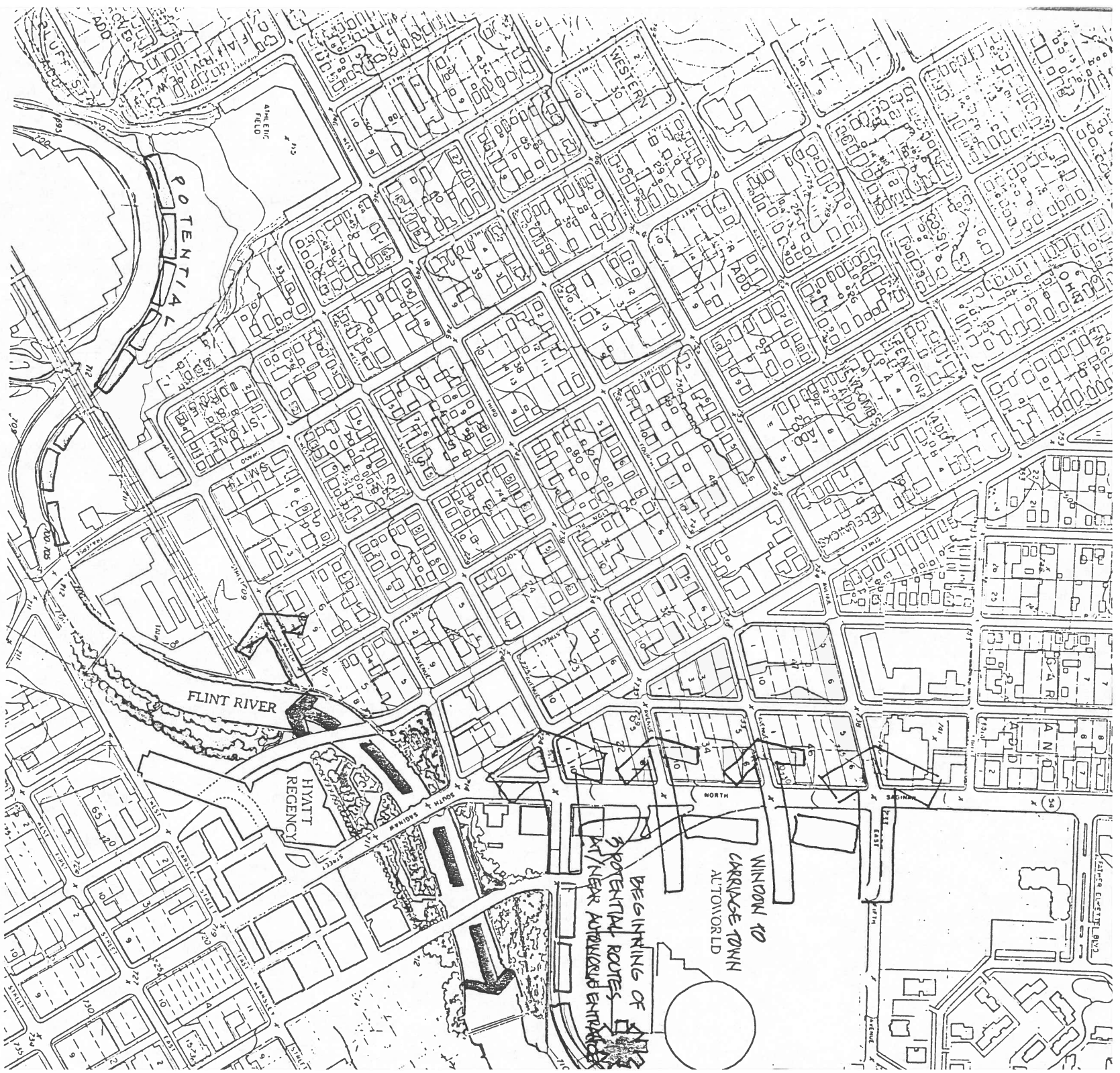


CULTURAL ATTRACTIONS - Sloan, Whiting, Peawaters etc.
 AUTOWORLD VISITOR
 HOTEL GUEST/CONVENTION TRADE
 RIVERFRONT SPECIAL EVENTS VISITOR
 ANWOOD STADIUM SPECIAL EVENTS VISITOR
 - A PHASED MARKETING/PROMOTION PROGRAM
 LEADING TO RECOGNITION OF CARRIAGE TOWN
 ON ITS OWN MERIT.

CARRIAGE TOWN
 OUT OF TOWN
 MARKET TO CAPTURE



Tomblinson, Harbun, York & Associates, Townscape Inc



PRIMARY LINKAGES FOR VISITOR/TOURIST

- RIVERFRONT PARK
- RIVER
- E-W STREETS (2nd, 3rd, 4th, 5th)

need to develop/reinforce N.S. deviation

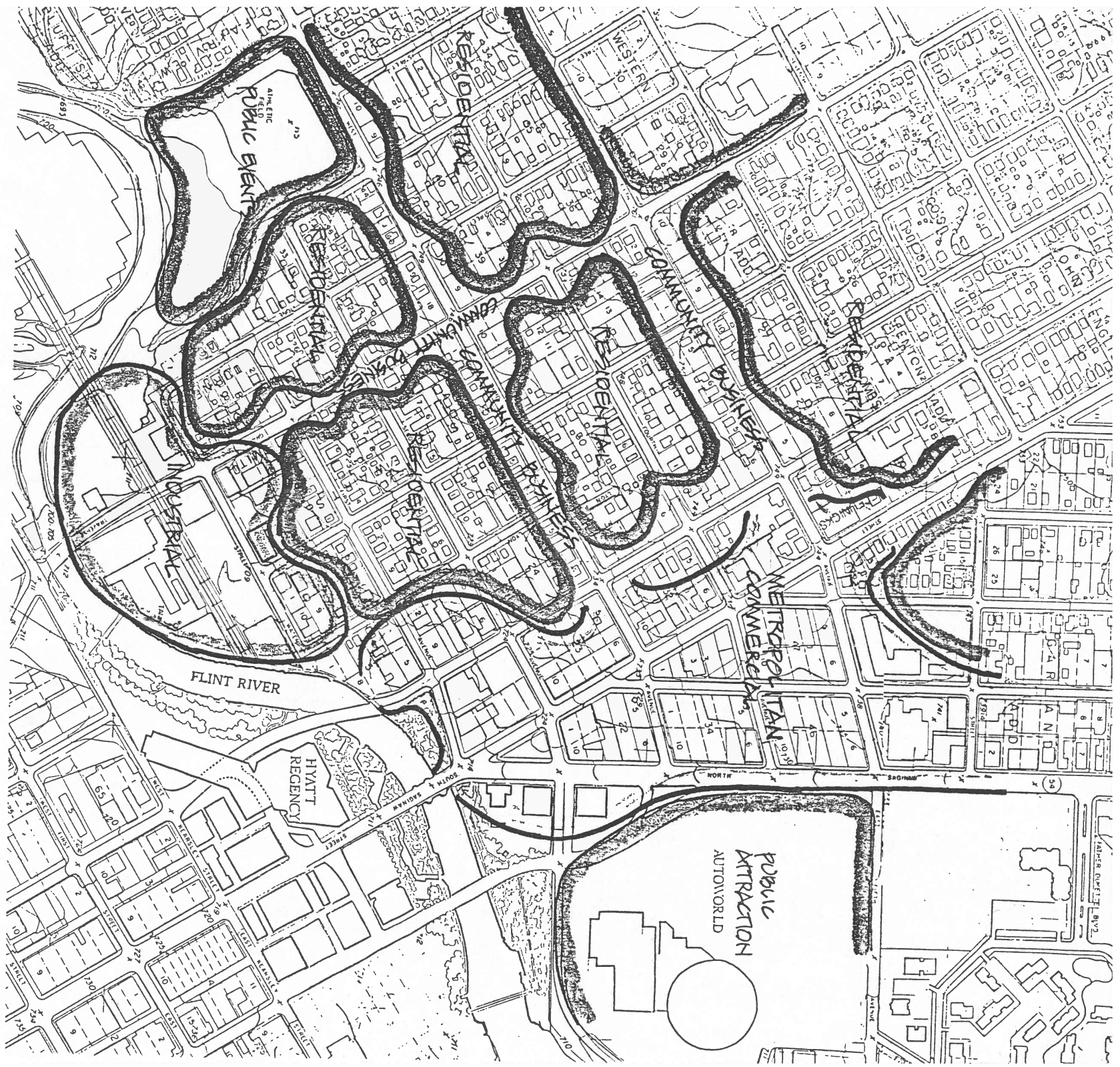
CARRIAGE TOWN LINKAGES



embusca urban, park & associates / townscape list

BEGINNING OF POTENTIAL ROUTES AT NEAR AIRBORNE ENTRANCE

WINDON 10 CARRIAGE TOWN ALTOVORLD

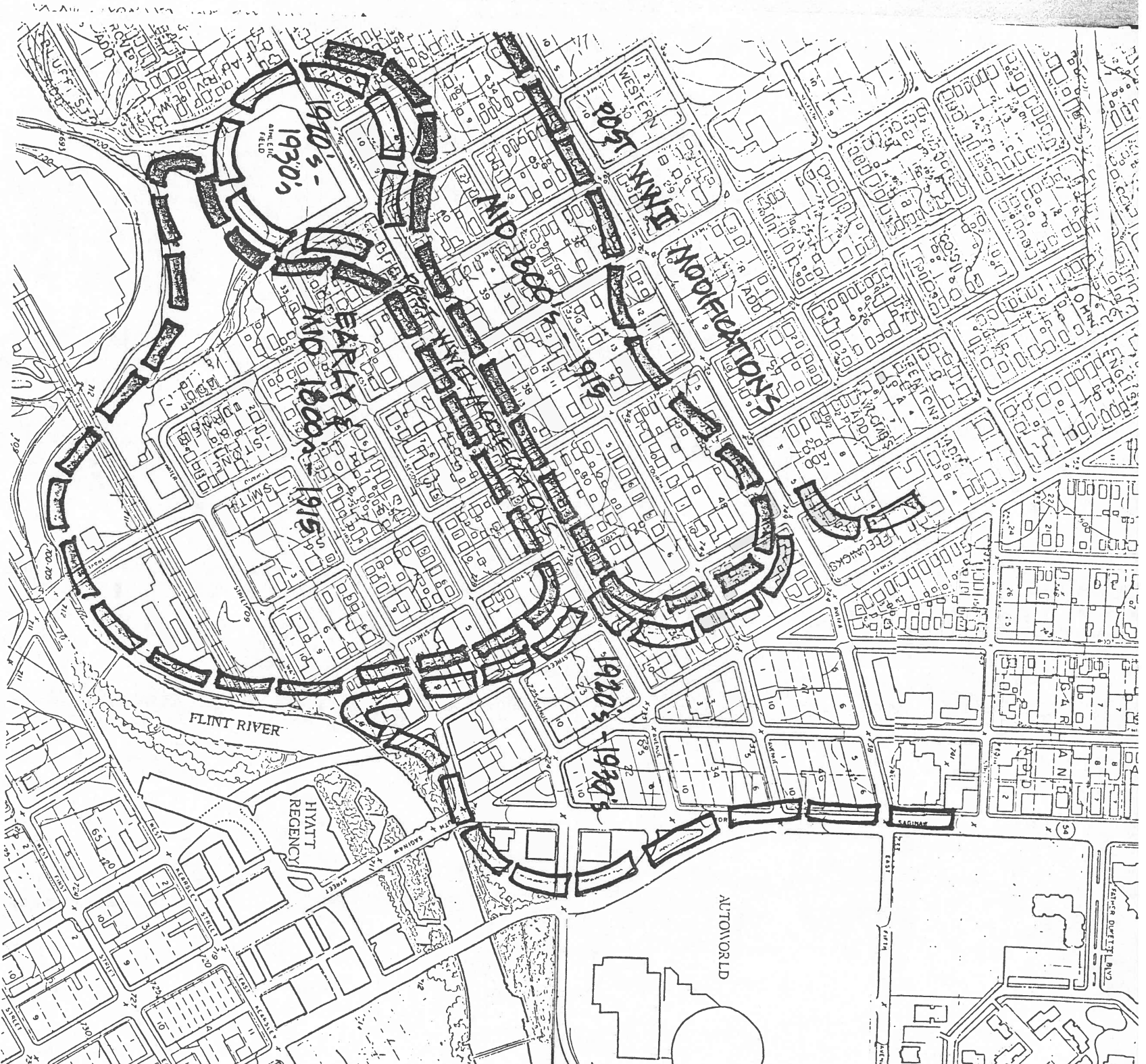


CARRIAGE TOWN

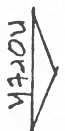
LAND-USE PATTERNS



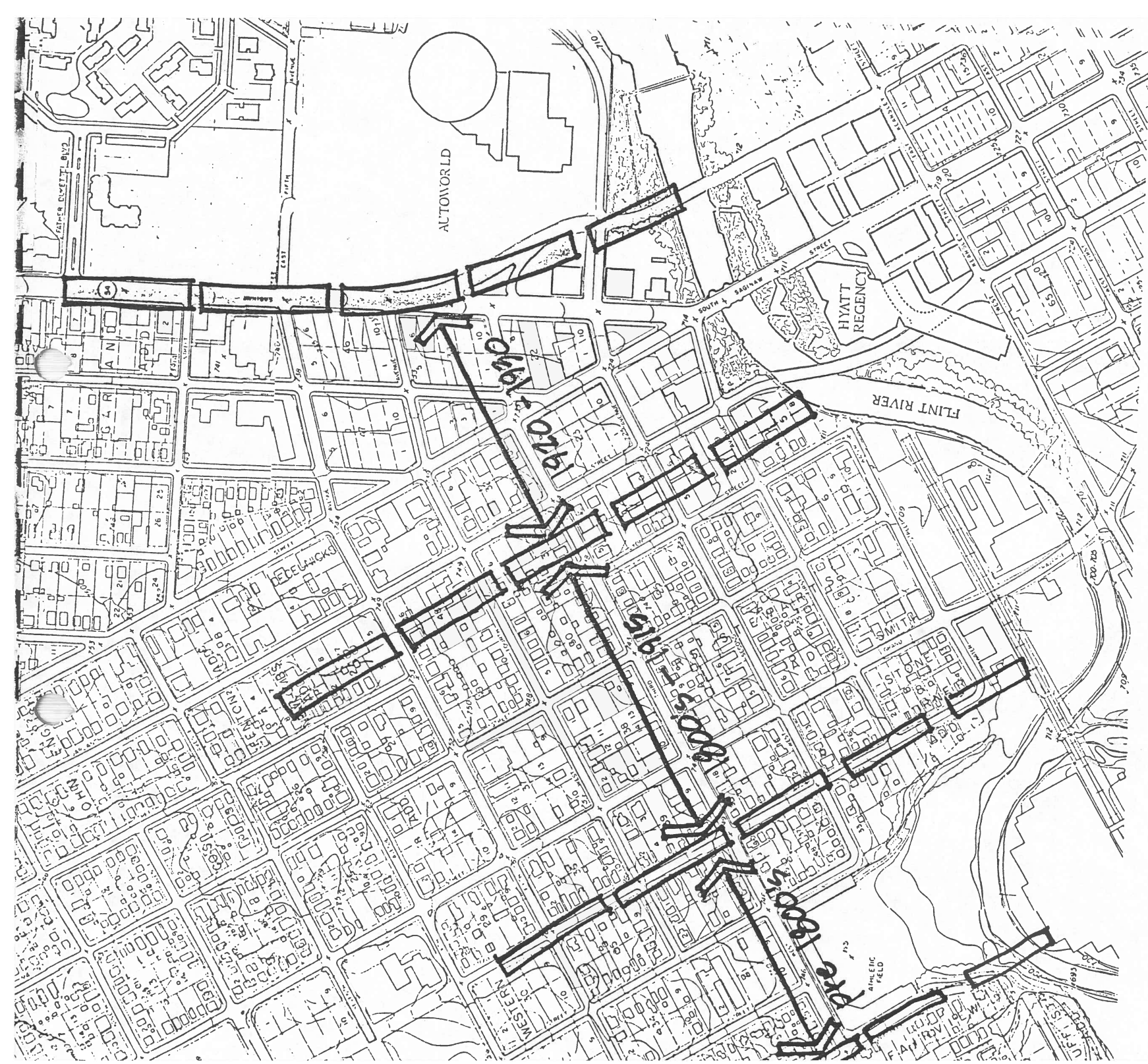
tomblinson, h. burn, york & associates / townscape inst



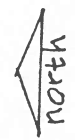
CARRIAGE TOWN
DEVELOPMENT PATTERNS



tombi . northern work associates / townscape inst

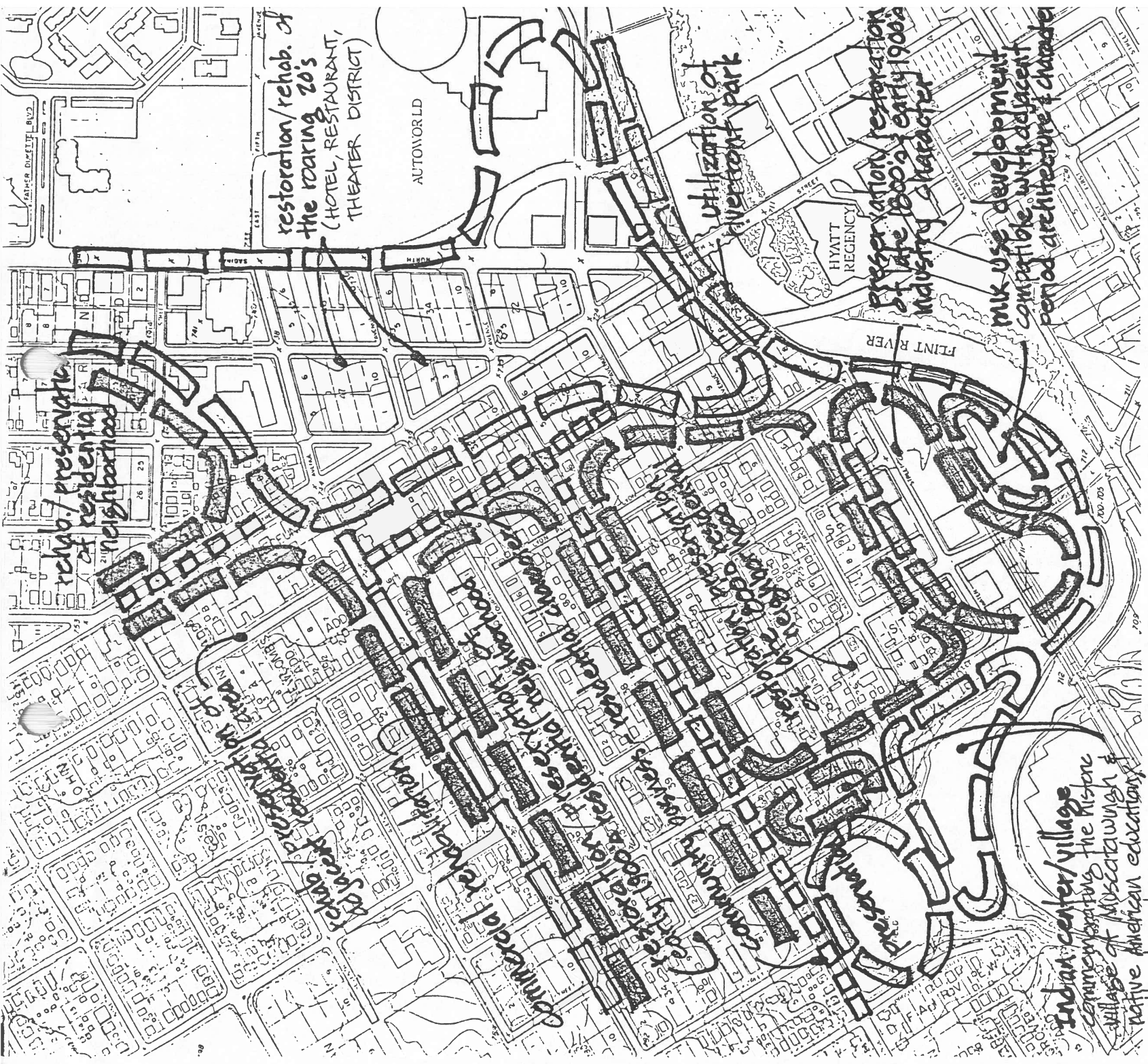


CARRIAGE TOWN



CONCEPT

tombinson, hartburn, york & associates / townscape list



restoration/rehab. of the roaring 20's (HOTEL, RESTAURANT, THEATER DISTRICT)

AUTOWORLD

UTILIZATION OF QUINCY PARK

preservation/restoration of late 1800's/early 1900's industry character

mix use development compatible with adjacent period architecture & character

rehab./preservation of residential neighborhoods

rehab/preservation of selected residential blocks

commercial rehabilitation

community preservation/residential neighborhood

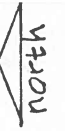
rehab/preservation of residential blocks

community preservation

preservation of residential blocks

Indian center/village commemorating the historic village of Muscatowung & native American educational cultural center

CARRIAGE TOWN



CONCEPTUAL PLAN

tombinson, hartburn, yuck & associates/ townscape inst